

# Hornsby Town Centre

Public Domain Guidelines

14/05/2024

COX



URBAN APOSTLES

MULTI-GENERATION CITY PLANNERS

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YOUR VISION.  
YOUR FUTURE.

Hornsby Town Centre



# QUALITY ASSURANCE

**Public Domain Plan**

Hornsby Town Centre  
c/o HPCR Project Team COX

**Prepared for**

Hornsby Shire Council

**Revision (see below)**

10

**Project Number**

219-0015-01

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**Issued**

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## 1.0 INTRODUCTION



# 1 Introduction

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‘This document is intended to supplement the Hornsby Town Centre Masterplan and is supported by background literature, opportunities and issues reports, stakeholder engagement summaries and technical reports prepared for Council as part of Hornsby Town Centre Masterplan. Strategies and improvements are outlined here to revitalise the public domain and support Council’s objective to transform the town centre in to a liveable, green and accessible centre that enhances public life for local community and its visitors.’

## 1.1 Aim of Public Domain Plan

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Improvement proposals for the Town Centre public domain are part of the overall urban design improvement strategy.

This has a focus on the scale of streets, potential for linkages both within the Town Centre and beyond to the broader community, and creation of a network of open spaces to provide larger and more meaningful places which can function at multiple levels to meet the needs of a diverse and evolving community including provision of event space. Improvements to public domain prioritise achieving streets and spaces which support pedestrian amenity, relate to the human scale and provide accessible paths of movement.

## 1.2 Design Philosophy

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Existing topographical, landform, vegetation and Town Centre character have been identified as being distinct local features. The Public Domain Structure Plan recommendations respond to these existing natural qualities, capitalizing on these aspects as having an advantage to shape precinct identity, landuse, movement corridors and materiality.

The Structure Plan creates a connected series of precincts which respond to character and reflect function, with the provision of new connections across the road and rail corridor, a cohesive approach to the Town Centre and establishment of vibrant and attractive destination.

Public domain strategies are identified which establish Open Space strategies, emphasizing green and blue networks to further reflect and strengthen these local assets.

## 1.3 Public Domain & Landscape Opportunities

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The following have been identified as being opportunities to be embedded into Public Domain proposals.

- Reinforce the “City in the Bush” experience
- Increase green streets and places to offer meaningful landscape amenity and promote active movement
- Provide public spaces for all ages and abilities
- Reinvigorate existing laneways as attractive spaces
- Provide trees with enough space to be viable green public domain
- Enhance legibility and connectivity
- Shape day and night attraction, identity, and safety
- Celebrate complex topography to enhance natural views and vistas while creating spatial interest and amenity
- Weave regional green networks into local connections, including Hornsby Park.

## 1.4 Document Structure

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The structure of this chapter comprises:

### 1.0 Introduction.

### 2.0 Understanding Hornsby Public Domain.

Analysis extracts which are pertinent to the understanding of the Public Domain in Hornsby and provide context for the basis of the public domain strategies and design principals.

### 3.0 Strategic Direction.

To support and guide implementation of the Public Domain Structure Plan with establishment of related networks to connect Green (vegetation), Blue (water) and Accessible and Liveable Public Open Spaces.

### 4.0 Public Domain Design Principles.

Description and explanation of the Public Domain Structure Plan.

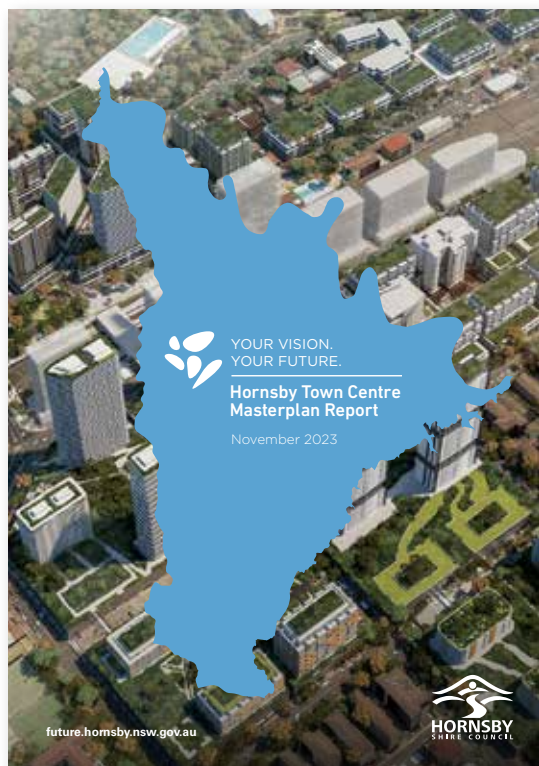
### 5.0 Public domain key places and elements.

Concept design of specific public domain places and elements including:

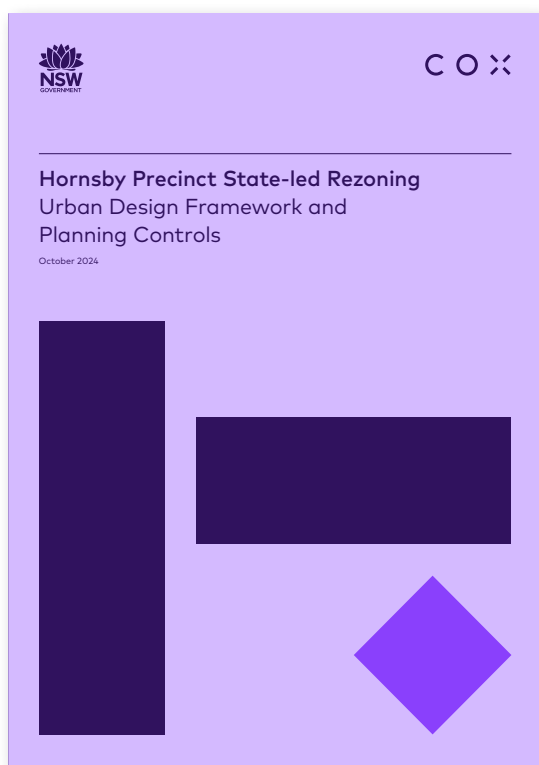
- Concept designs for key public domain areas
- Plans and sections of typical street typologies
- Material palette for furniture, vegetation, lighting, paving.

## 1.5 Background Documents

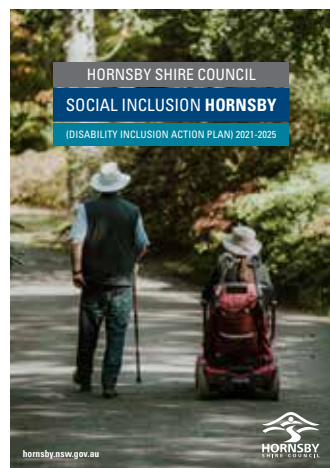
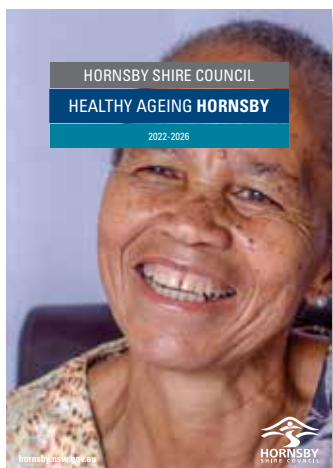
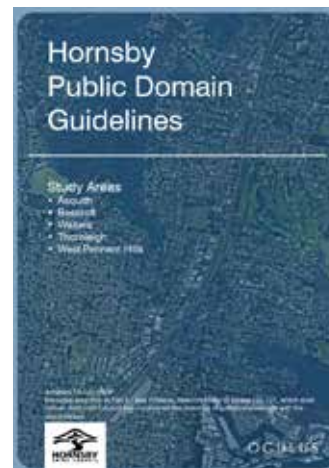
These documents have been used to inform and direct proposals outlined in this report.



- Hornsby Town Centre Review: Landscape and Public Domain
- Hornsby Town Centre Review: Transport Plan
- Hornsby Town Centre: Net Zero Carbon Precinct Strategy for Building Design
- Economic Development and Employment Land Uses
- Hornsby Town Centre Review: Stage 2 Stakeholder Engagement Outcomes Report
- Draft Hornsby Town Centre Consultation Feedback Report



- Hornsby Precinct State-led Rezoning: Urban Design Framework and Planning Controls
- Hornsby Precinct Design Guide
- Hornsby Infrastructure Delivery Plan









## 2.0 UNDERSTANDING HORNSBY PUBLIC DOMAIN

## 2 Understanding Hornsby Public Domain

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### 2.1 Vegetation Analysis

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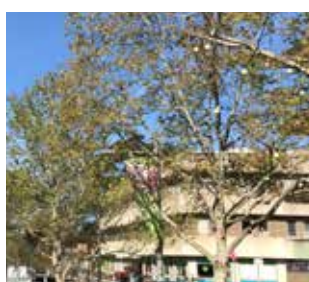
Hornsby is a city surrounded by native vegetation. Fingers of vegetation continue into the undeveloped valleys and bring the landscape close to the town centre on the west. These spaces are preserved as part of the open space network including Hornsby Quarry, Hornsby Park and Lisgar Gardens and also include vegetation within the streetscape or yards of suburban development.

The vegetation is dominated by the Blackbutt Gully Forest community to the west of the rail line. Isolated remnants within the streetscape and subdivision have not been identified.

Street tree planting is limited and inconsistent both within and beyond the study area. Streetscapes would benefit from more continuous planted treatments where conditions allow.

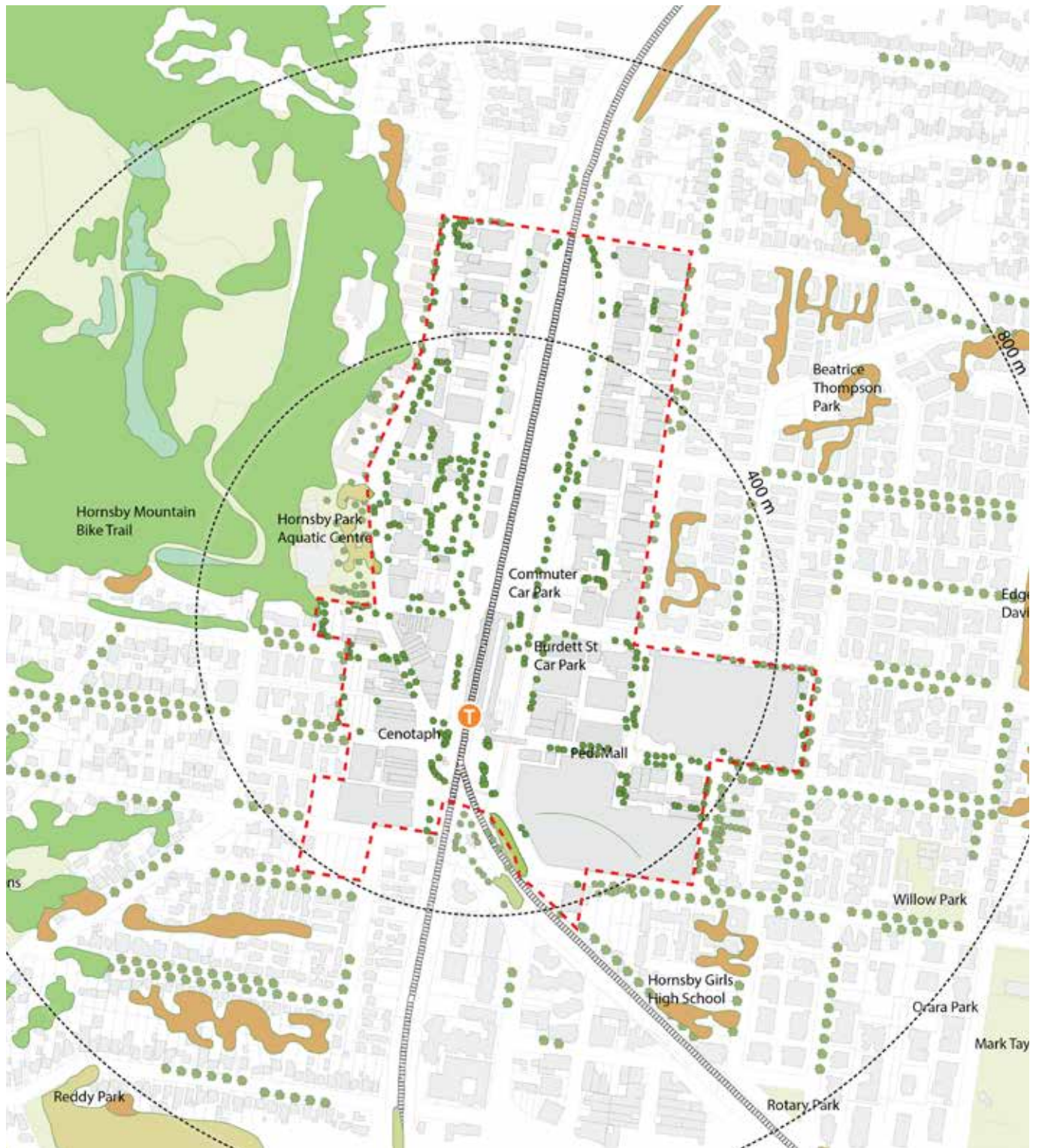
Active shopping streets such as the Hornsby Mall - Florence Street, and Peats Ferry Road have adopted a deciduous palette including Zelkova and Platanus (Plane Tree) species.

Other streets have adopted a predominantly native plant palette comprising *Lophostemon confertus* (Brush Box), *Corymbia maculata* (Spotted gum) and *Angophora costata* (Sydney red gum).

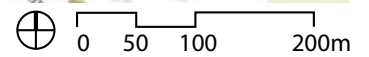


Top Left: *Angophora costata* - Sakadang Kuya  
Top Right: Black Butt - Watersmeet  
Middle Left: Plane Trees - Tract  
Middle Right: Zelkova - Tract  
Bottom Left: Brushbox - Tract  
Bottom Right: Spotted Gum - Katanning Landcare





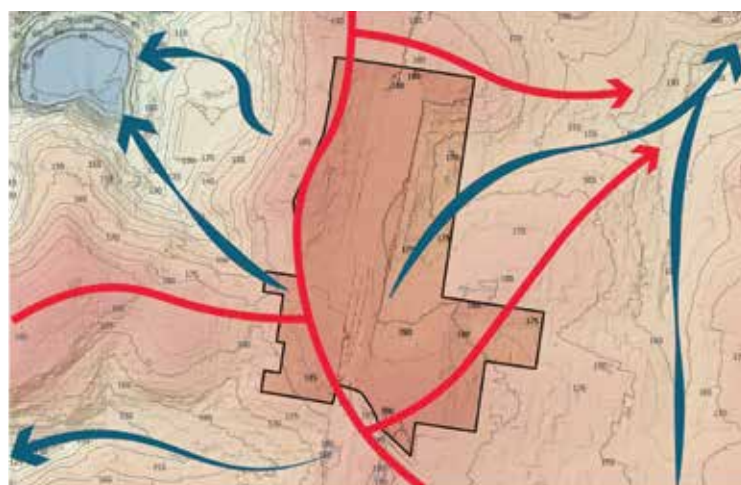
Key Plan



KEY

- |                        |                            |
|------------------------|----------------------------|
| Blackbutt Gully Forest | Street Tree                |
| Blue Gum Diatreme      | Blue Gum Shale Forest      |
| Open Space             | Study Area                 |
| Remnant Vegetation     | Turpentine Ironbark Forest |

## 2.2 Topology & Hydrology Analysis



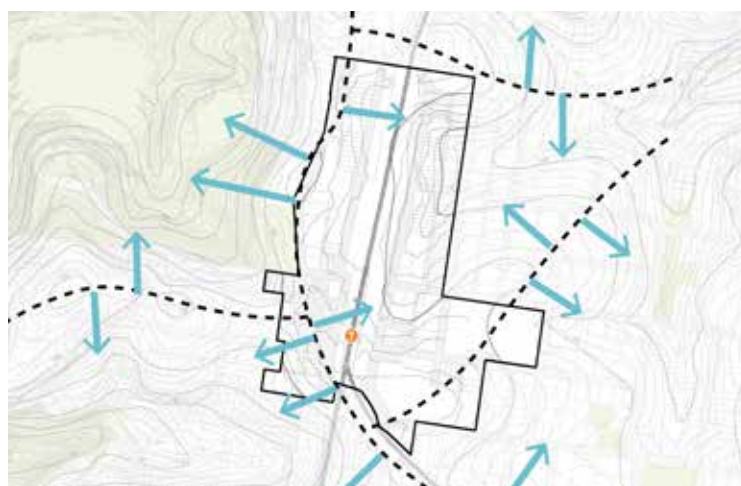
KEY  
— Drainage Line  
— Ridgeline

Hornsby Town Centre is situated at the top of a ridge. Peats Ferry Road follows the ridgeline and is a significant remnant of a movement line through the area that likely dates from pre-settlement times.

With Hornsby being developed on the plateau of the ridge it allows for views down into the largely undeveloped valley. The west side of the train line is relatively flat whilst the east side is undulating and slopes down further east. The high point of the site on the west side is 195m above sea level. Parts on Hunter Street on the east side drop down to 165m.

This topography gives the Town Centre unique vantage points which can be manipulated in the structure plan design. Many of the streets have long vistas which should be maintained and enhanced.

This elevated position is unique within the Sydney Metropolitan Region and is reflected in that the Town Centre drains in to three distinct water catchments of Berowra Creek and Cowan Creek, north to the Hawkesbury River and the Lane Cove River to the south.

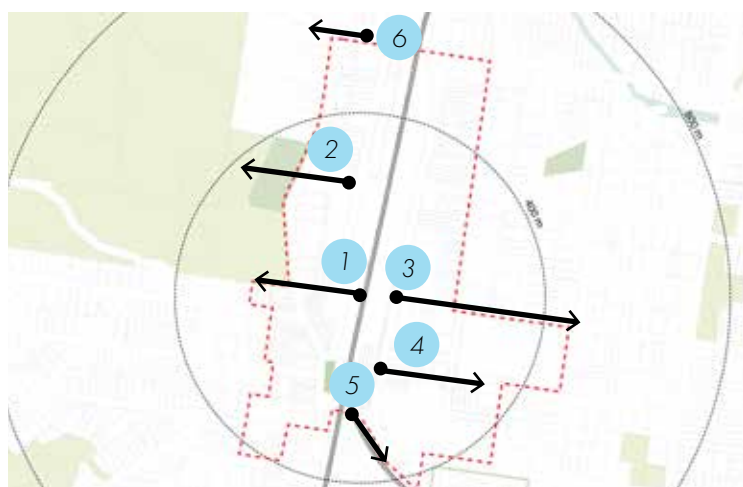


KEY  
--- Ridgeline  
→ Water flow direction



## 2.3 Vantage Points and Key Views

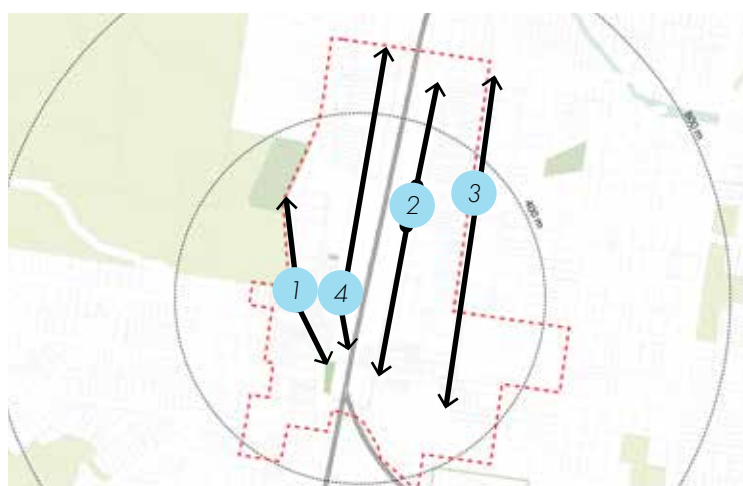
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Key view corridors are recognised and can be emphasised to acknowledge the opportunity for views out of the town centre, towards surrounding bushland. Emphasising these contributes to the creation of the town character.

These views are:

1. Coronation Street views west
2. Peats Ferry Road west
3. Burdett Street views east
4. Florence Street views east
5. Peats Ferry Road south
6. Bridge Street views east



Where topography and street alignment contains and constrains views, the experience of Hornsby Town Centre will continue to be of a well vegetated streetscape environment.

These views are:

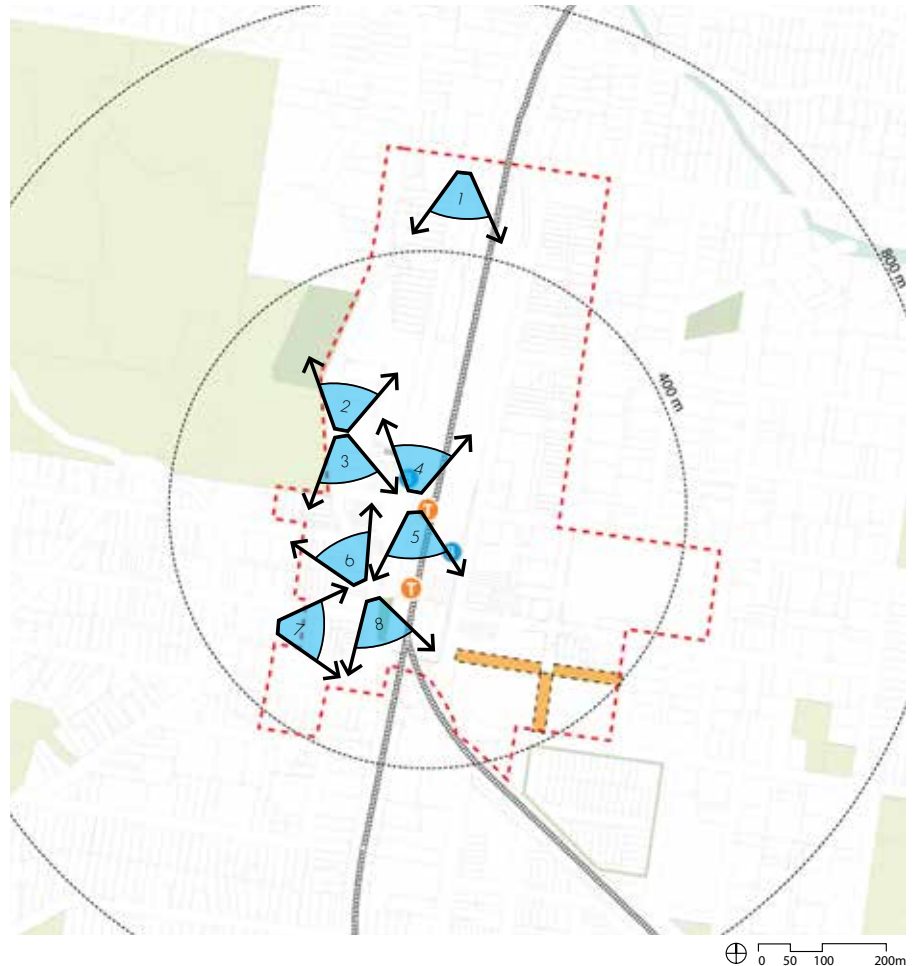
1. Peats Ferry Road views north and south
2. George Street views north and south
3. Hunter Street north and south
4. Jersey Street

## 2.4 Street Character - West

West of the rail line, the town character is reflected in a street frontage of European heritage buildings, retail and civic uses. Further from the centre, civic buildings transition into large villas with generous tree lined front garden spaces. This provides visual interest for the public domain experience with variety in built form, height and street corridor scale.

There are a number of notable civic brick buildings in this area which have influenced the recently upgraded footpath surface. The transport interchange area disrupts the character being utilitarian in its presentation. It is a large expanse of vehicle dominated surfacing with minimal pedestrian amenity beyond waiting shelters.

There are numerous established trees which provide a leafy canopy streetscape character. All views out of the Town Centre's west side are dominated by bushland trees.



Key Plan



1



2



3



4



5



6



7



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## 2.5 Street Character - East

Generally to the east of the transport corridor, public domain amenity is focused upon the retail shopping mall area in the core of the town centre. Built form is larger and there is less contrast of public domain streets and spaces.

George Street is a fast moving vehicle dominated environment which creates an uninviting pedestrian experience and adds to the barrier effect of the transport corridor between east and west.

Some streets are tree lined and most residential properties have a verge space suitable for increasing the presence of street trees and connected vegetation corridors.







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## 2.6 Study of Open Spaces

Catchment areas of existing parks were examined to assess a 200m walking zone from existing parks to assist with justification of where future open spaces should be located in the Town Centre Structure Plan. This aligns with “Greener Public Spaces Premier’s Priority” and considers the accessibility of these spaces from residential areas.

The study area includes areas beyond the project scope to ensure a holistic

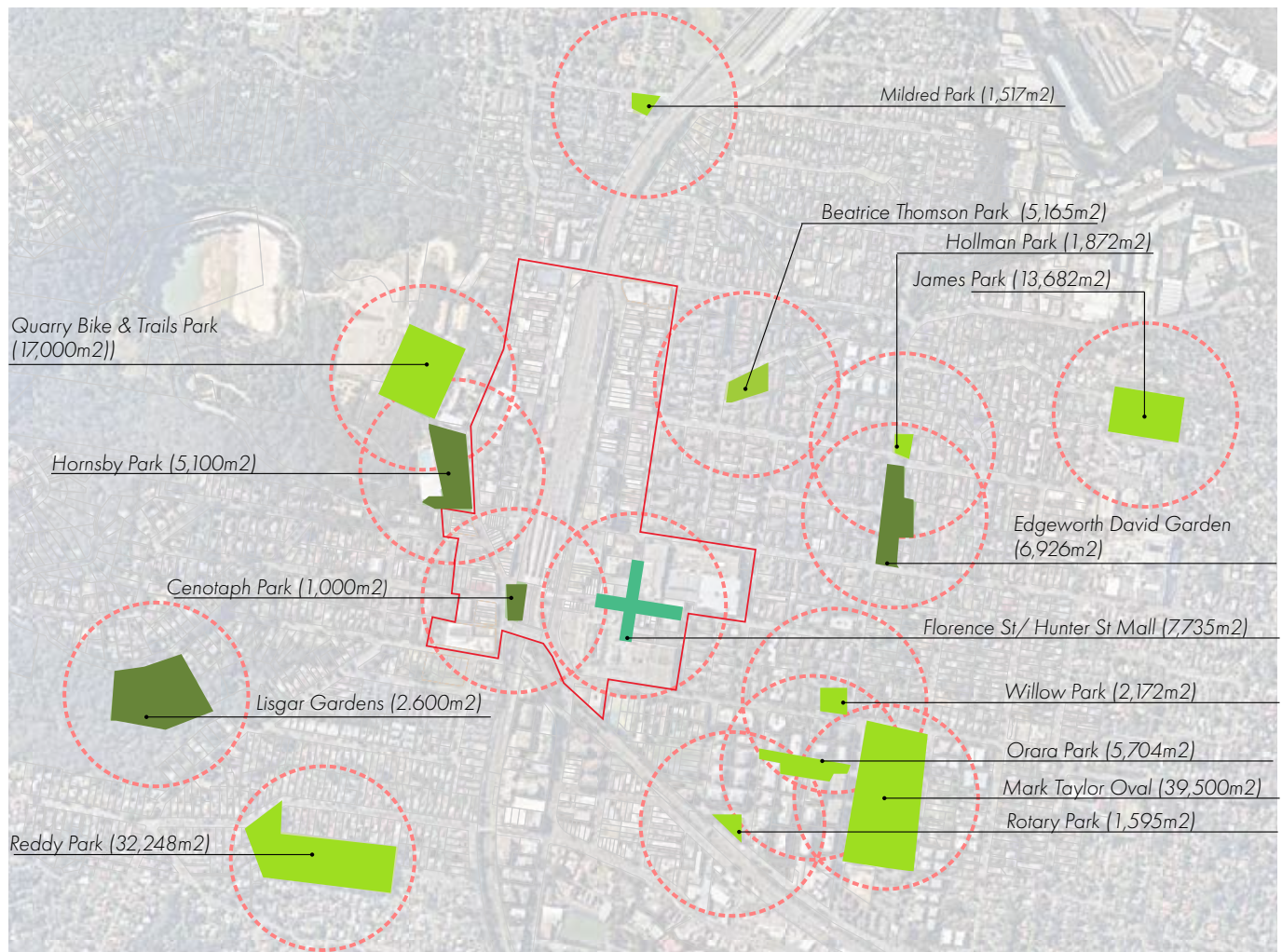
approach to understand the existing provision and context for town centre open space.

The study shows that there is a notable east west division, with the east town centre generally having very little open space currently.

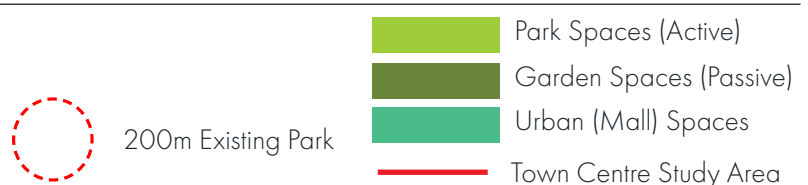
In the east of the study area, Florence Street shopping mall and laneways are included in the assessment, as they are an active and valuable area of public domain, (although noted as being a pedestrianised mall area and

not a traditional open space park type place).

With the introduction of new residential dwellings to the town centre generally, the study indicates that provision of more open space, particularly to fill the notable gaps in the east town centre, is warranted to ensure future developments fulfil the Premier’s priority for access to open space.



### 200m Catchment Of Existing Open Spaces





The provision of two new spaces within the Town Centre study area are recommended to address the gap as revealed through the investigations of the existing open spaces.

Increasing the areas of existing open space will further assist with improving the availability of open space:

- New open space provision at Jersey Street and Burdett Street increase the accessibility of open space for future residents of the town centre

- Enlarging the existing park space at Cenotaph Plaza, and providing additional urban plaza at Hornsby Square will increase the open space available for future residents.

Provision of new open spaces should reflect the directions, principles and elements of this document. They should also incorporate Council's open space policies, including:

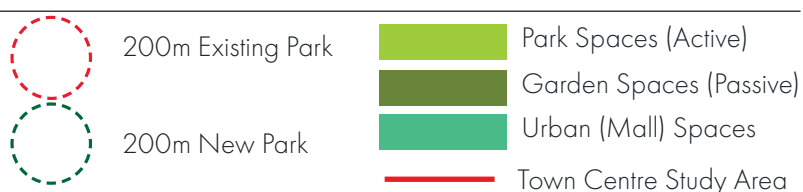
- Hornsby Public Domain Guidelines
- Healthy Ageing Hornsby
- Social inclusion Hornsby

Spaces should be programmed deliberately to work as part of the open space network:

- In relation to context and each other as companion places
- Future user needs and requirements;
- Functional requirements for the town centre; and
- Consideration for connected open spaces for amenity and ecology.



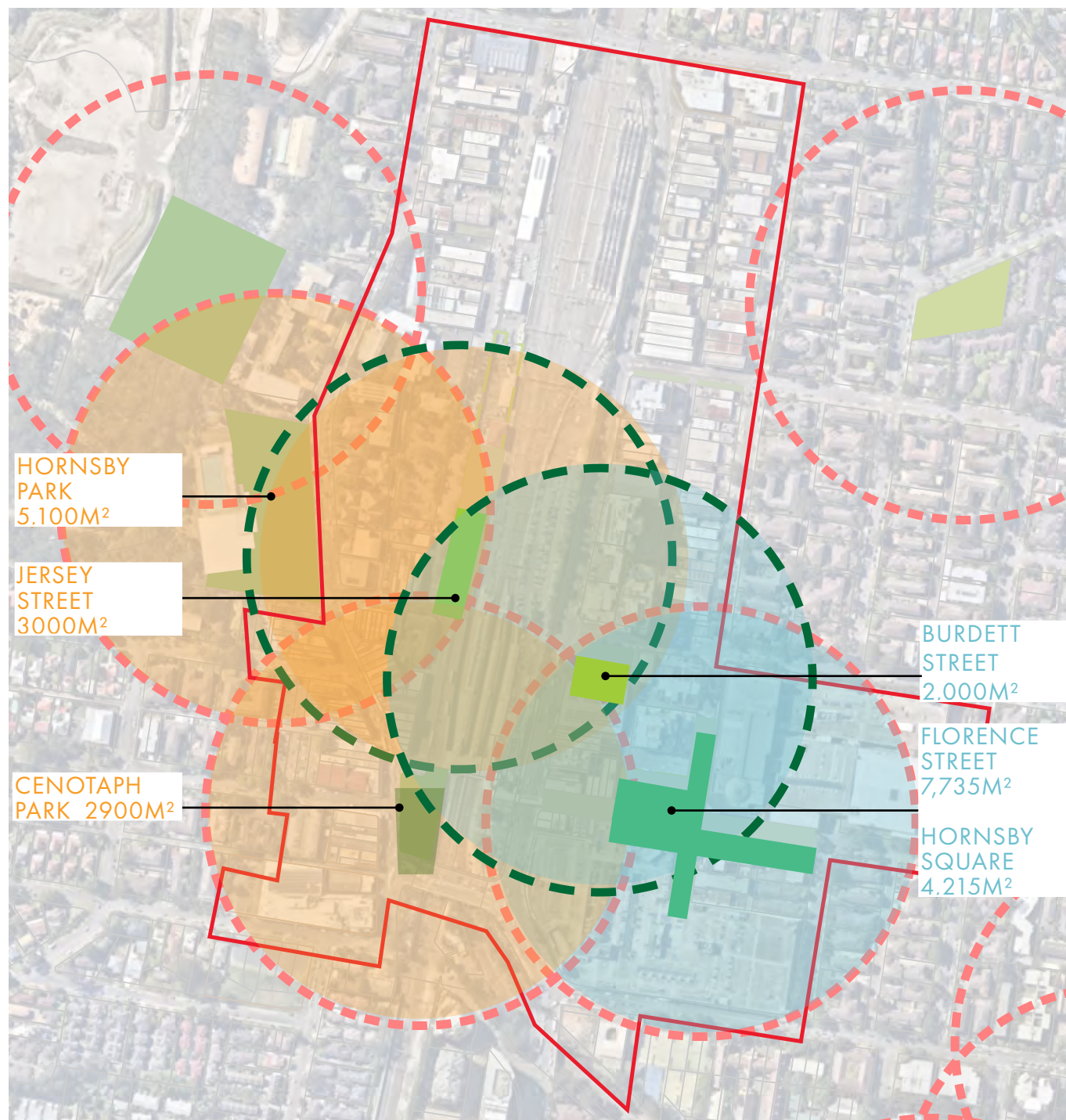
## 200m Catchment Proposed And Existing Open Spaces



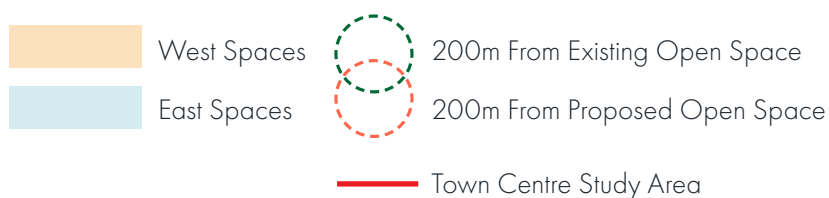
## 2.7 Study of Open Spaces - Town Centre Detail Assessment

Two new open spaces at Jersey Street and Burdett Street have 200m distance catchment zones which generously overlap with existing

spaces, supporting accessibility, and availability, to both east and west sides of the rail corridor.



200m Catchment Of Open Spaces





The quality of these open spaces is important for the town centre, however quantity of m<sup>2</sup> per person assessments are also investigated. Based on these identified town centre open spaces and the likely population projection of 179,582 by 2031, there would be approximately 1.7m<sup>2</sup> of open space per person.

This is examined in relation to east/west provision below.



### West Open Space Review

11,000 m<sup>2</sup> Approx open space

All potential dwellings are within 200m of open space

Jersey Street 3,000m <sup>2</sup> (New)	Active space, town play space, adaptive heritage building reuse, civic uses, youth spaces multi sport.
Cenotaph Plaza 2,900m <sup>2</sup> (Existing increased)	Connection to country, restful contemplative, community events, ceremonial events, RSL connection.
Hornsby Park 5,100m <sup>2</sup> (Existing, outside town study area)	Garden, restful, play, town heritage.
Population	1,974 people*
Open space per person	5.57m <sup>2</sup>



### East Open Space Review

13,950 m<sup>2</sup> Approx open space

All potential dwellings are within 200m of open space

Burdett Street Park 2,000m <sup>2</sup> (New)	Garden park, play, rest, stay, connection to country.
Hornsby Square 4,215m <sup>2</sup> (New)	Urban square, retail meeting and resting place, pop up screen events, outdoor eating, community events, organised events.
Florence Street 7,735m <sup>2</sup> (Existing)	
Population	9,471 people*
Open space per person	1.47m <sup>2</sup>

Note \* population numbers see supporting Hornsby Town Centre Review documents for further detail







## 3.0

### STRATEGIC DIRECTION

#### THREE BIG MOVES

- GREEN & BLUE NETWORK
- ACCESSIBLE STREETS FOR PEOPLE
- LIVEABLE PLACES FOR PEOPLE



## 3.0 Strategic Direction

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### 3.1 Green & Blue Network

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Green and Blue Networks are a way of planning, based around waterways (Blue), and planting and parks (Green). These are managed together through a combination of infrastructure, ecological restoration and urban design to connect people across the city.

They provide an integrated green and blue network of connecting streets, bushland and open spaces, that will improve public amenity and reflect a city in the bushland vision.

- Prepare a street tree masterplan that will reinforce hierarchy of the street network and improve the urban ecology.
- Integrate Water Sensitive Urban Design into the new designs.
- Share the water story knowledge through interpretive features and to create a more visually interesting and culturally diverse public domain.



### 3.2 Accessible Streets for People

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Create pedestrian and cycle-friendly street environments. Prioritise people, by designing an environment where priority is given to pedestrian presence and cycling infrastructure.

- Ensure streets and intersections are designed for pedestrian priority.
- Reduce traffic speed in high volume pedestrian areas.
- Improve physical and visual access through safe, activated and well connected streets and laneways.
- Improve footpath amenity.



### 3.3 Livable Places for People

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Offer places with a diverse range of year-round uses, high public amenity and distinct design quality, where the stories of Hornsby are represented and celebrated.

- Revitalise the character of the Town Centre with enhancement of existing qualities and celebrate Indigenous and European heritage.
- Achieve desirable and inviting public open spaces with high level of amenity.
- Provide a variety of spaces that are inclusive of diverse needs and desires of different groups of people.
- Create an adaptable and flexible public domain, capable of accommodating a broad range of uses and events.





## 4.0 PUBLIC DOMAIN DESIGN PRINCIPLES

## 4.0 Public Domain Design Principles

### 4.1 Environment & Ecology Overlay - Local Green Grid Network

Green grids highlight the unique and distinct local qualities of topography and vegetation to create a framework for public domain streets and open spaces.

Green infrastructure builds connected vegetation communities, has visual

significance to relate to bushland surrounds plus provides comfort in the form of improved microclimate, access to shade and shelter.

The green network responds to and provides:

- A relationship with the ridge line, slopes and gullies
- A visual connection to context bushland.

- Ecological connections, to provide a network of vegetation corridors.
- Relationship with the water story of the place and connection to the district networks.



### Green Infrastructure Network



## 4.2 Environment & Ecology Overlay - Blue Network

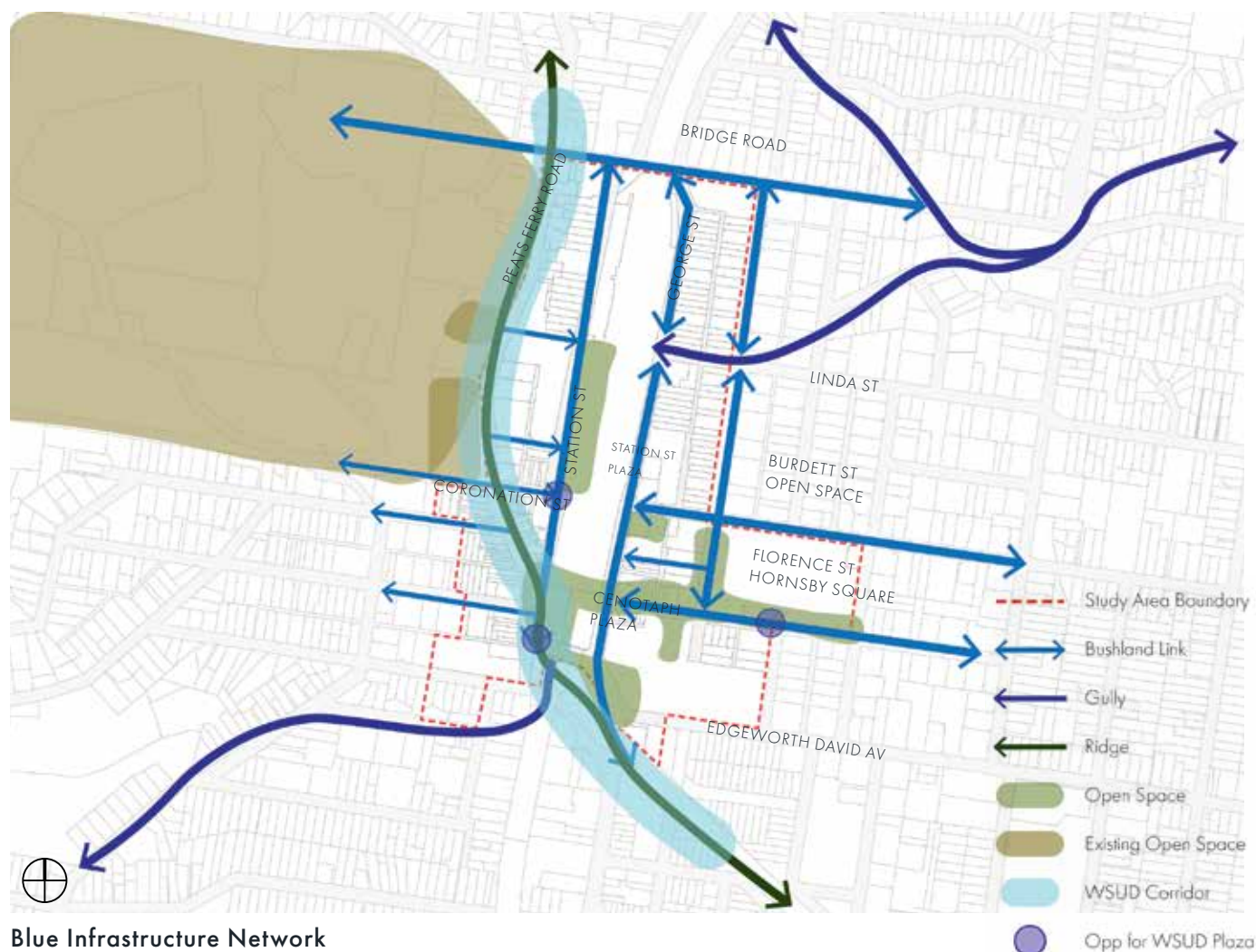
Blue infrastructure responds to topography, integrating drainage and stormwater needs into a deliberate and designed network which informs the character of the streets and public open spaces.

In conjunction with the green network and aligned with Hornsby Shire Council Water Sensitive Urban Design (WSUD) Strategies, the blue network

will play a major role in providing functional landscapes in the Town Centre.

The development of a Blue network local drainage strategy enables the interpretation of the local water story and reflects the topography of the place. The key functions of the network can be summarised as:

- Ridgelines form an important and symbolic movement line for this area and a starting point for representing water story.
- The Movement of water along streets to plazas which can play a role with WSUD collections points.
- WSUD Streets form a connected and functional landscape network
- Gullies are the final connections to the broader blue network, their location can help inform street typologies and urban form.
- Align WSUD network with green corridors to support ecology networks



### 4.3 Biodiversity Corridors

Hornsby Shire Council Biodiversity Strategy identifies a number of corridors to focus on the provision of biodiversity initiatives, which are embedded into Public Domain proposals.

A number of corridors in the town centre have been identified as Biodiversity Corridors. These are-

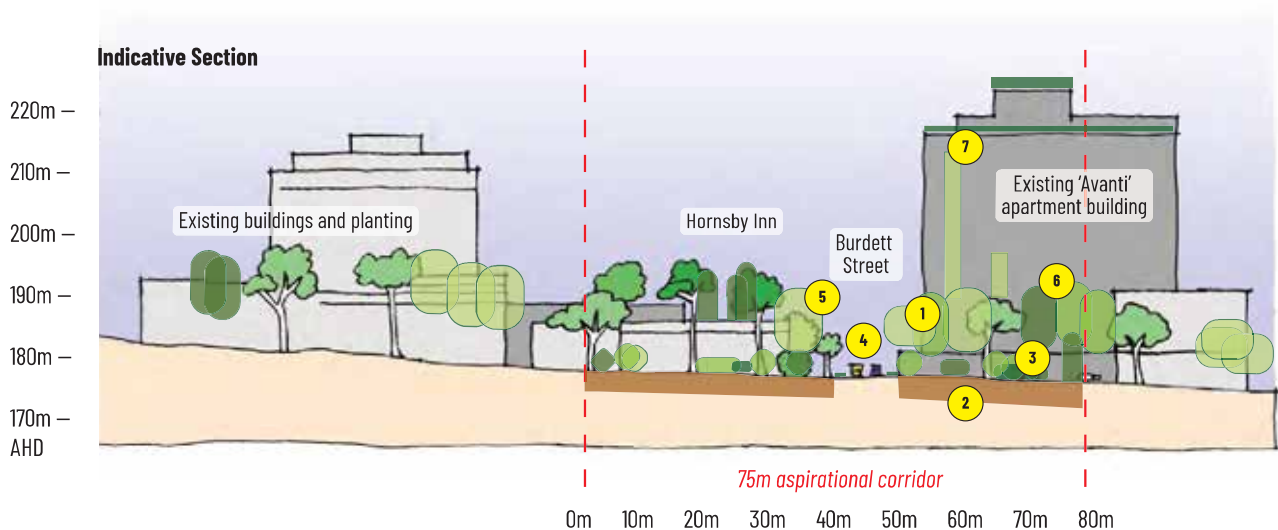
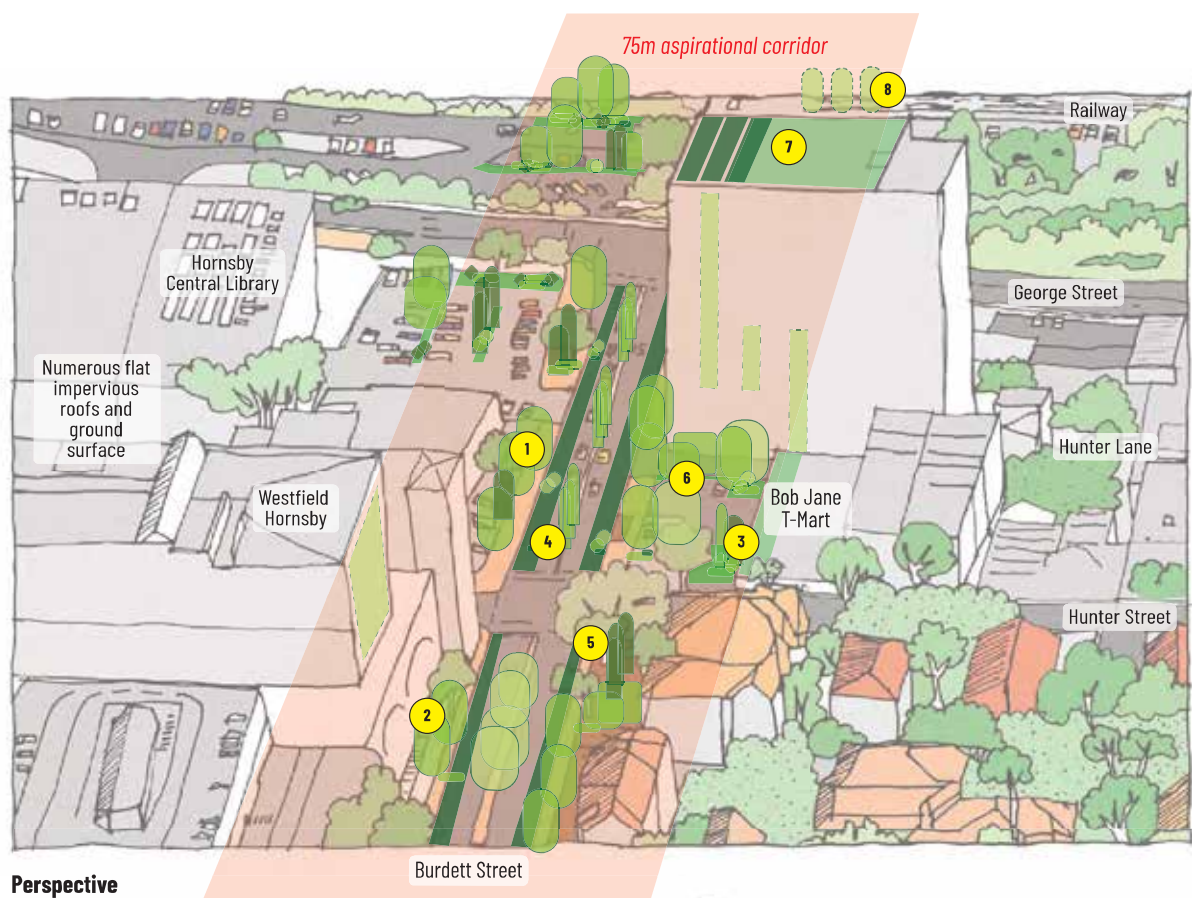
BURDETT ST	(75m wide)
PEATS FERRY RD	(50m wide)
RAIL CORRIDOR	(50m wide)
BRIDGE RD	(20m wide)
EDGEWORTH DAVID RD	(20m wide)
SHERBROOK RD	(20m wide)

Within these corridors, the following is recommended:

- Establishment of a street tree masterplan, promote interconnected tree canopies and increase the urban forest;
- Target road narrowing with installation of WSUD, to include additional shade trees and ground cover vegetation;
- Selection of porous surface materials to replace impervious ones;
- Promote vegetation species selection which considers micro and macro habitat for urban wildlife;
- Consider undergrounding of overhead power cables to encourage canopy growth; and
- Inclusion of green walls and roofs where possible.
- Consider proposals which bridge over rail and road infrastructure barriers.



### Biodiversity Network



- 1 Interconnect street canopy and support biodiversity by filling vacant street tree sites with mix of species appropriate for highly constrained urban environment and retaining and actively managing habitat trees via street tree planting plan
- 2 Protect and enhance front setbacks through DCP and apply deep soil requirements for commercial and med-high residential supported by solid landscape character statement
- 3 Select replacement of carparks, driveways and other impervious surfaces with additional trees and vegetation and increase opportunities for deep soil/associated planting zones
- 4
- 5 Work with AusGrid for aerial cable bundling/undergrounding of powerlines to facilitate urban canopy growth
- 6 Potential to collaborate with major landholders and community groups to increase urban forest
- 7 Green roofs/walls and bird friendly building design will be encouraged through the development of relevant Council policy
- 8 Work with Sydney trains to identify parcels of land that support and maintain for biodiversity and rail safety outcomes

Extract Hornsby Shire Council Biodiversity Conservation Management Plan:  
Fig 54: Green Infrastructure Framework Visualisation: 75metre "aspirational" corridor

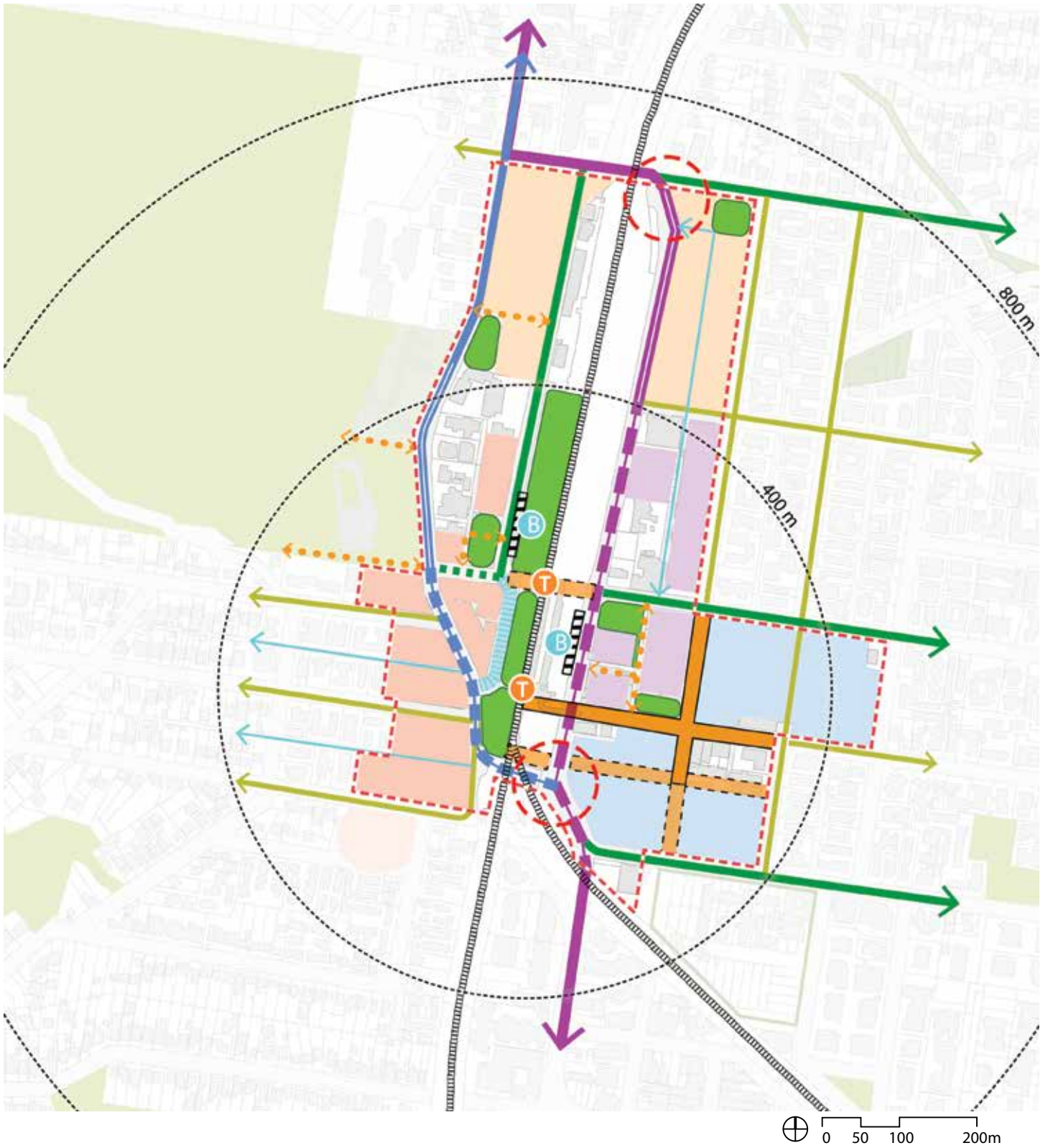


## 4.4 Public Domain Structure Plan

The public domain structure plan arranges a hierarchy of street typologies and open spaces in response to:

- Provision of connected green and blue networks throughout the town centre
- Requirement for improved and well connected public domain within the town centre with pedestrian focus.
- The needs of each precinct and the function/ type of activity to be accommodated and encouraged within the town centre.

	Study Area Boundary
	Gateway
	Public Open Space
	Pedestrianised Mall / Potential Pedestrianised Zone
	Pedestrian Link / Laneway
	Slow Zone / Shared Road (One Way) with Primary Green/Blue Network Application
	Slow Zone / Pedestrian Priority Road
	Medium Zone / Local Street with Secondary Green/Blue Network Application
	Medium Zone / Local Street with Primary Green/Blue Network Application
	Medium Zone / Local Street with Town Centre Treatment & Primary Green/Blue Network Application
	Medium Zone / Collector Road with Town Centre Treatment and Primary Green/Blue Network Application
	Medium Zone / Collector Road with Primary Green/Blue Network Application
	Periphery Zone / Collector Road with Primary Green/Blue Network Application
	Medium Zone / Arterial Road with Town Centre Treatment and Primary Green/Blue Network Application
	Periphery Zone / Arterial Road













## 4.5 Street Typology Overlay

Streets are the fundamental elements of the public domain.

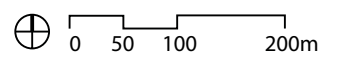
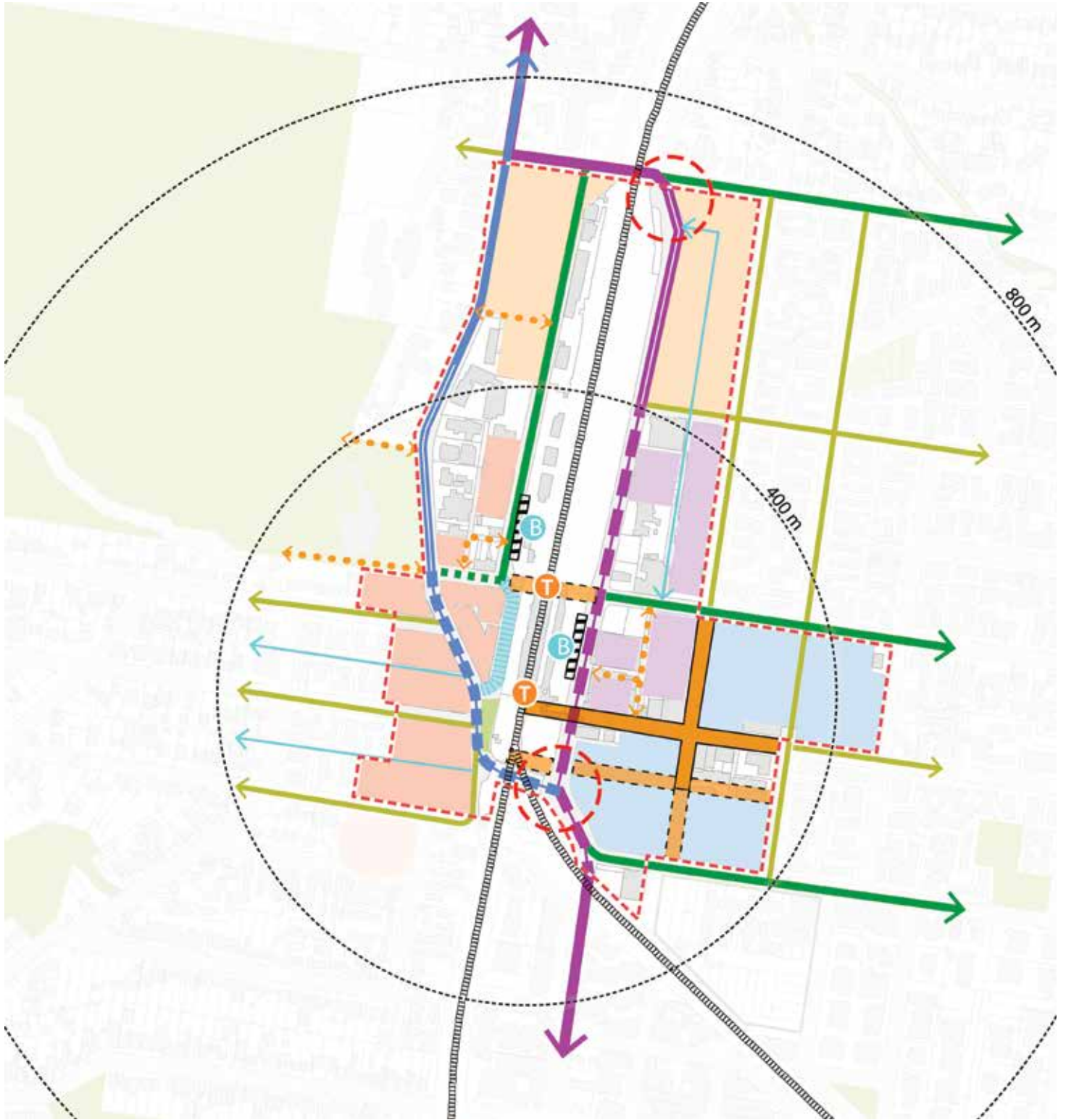
A clear street typology reflects the street hierarchy and defines the street character. It is critical in creating a liveable and sustainable urban environment and is important for people to inform and orient themselves around the town centre.

Street typologies are assigned to zones to promote pedestrian/cycle friendly environment:

- Prioritising pedestrians with slow-medium speed traffic in the Town Centre Core;
- Marginalising fast traffic to the Town Centre periphery.

	Slow Zone /Shared Road (One Way) with Primary Green/Blue Network Application
	Slow Zone /Pedestrian Priority Road
	Medium Zone /Local Street with Secondary Green/Blue Network Application
	Medium Zone /Local Street with Primary Green/Blue Network Application
	Medium Zone /Local Street with Town Centre Treatment & Primary Green/Blue Network Application
	Medium Zone /Collector Road with Town Centre Treatment and Primary Green/Blue Network Application
	Medium Zone /Collector Road with Primary Green/Blue Network Application
	Periphery Zone /Collector Road with Primary Green/Blue Network Application
	Medium Zone /Arterial Road with Town Centre Treatment and Primary Green/Blue Network Application
	Periphery Zone /Arterial Road





## 4.6 Public Domain - Open Spaces & Activation Overlay

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Regular community events and activation of the public domain provide social and economic benefits, i.e. supporting the local economy and contributing to the identity of place and community.

Public domain activation opportunities include:

- Gateway;
- Public Open Spaces;
- Pedestrian Mall; and
- Pedestrian Links/Laneways.

Three town centre Key public places have been identified as holding significant roles within the open space network, being:

1. Peats Ferry Road - Cenotaph Plaza: a central park at Gateway location;
2. Jersey Lane - Active Community Plaza and public open space;
3. Hornsby Square - Urban Plaza; and
4. Burdett Street Park - Passive green space













## 5.0 PUBLIC DOMAIN KEY PLACES & ELEMENTS

## 5.0 Public Domain Key Places & Elements

### 5.1 Open Space Network

The provision of a series of connected open spaces are arranged within the town centre.

These are linked via walkable streets and increased over rail connections. These spaces will have a variety

of scale, nature, programming and functions to support the range of needs of the increased Town Centre population. The network will:

- Increase provision of open space to support development proposals
- Provide a network of linked open space and vegetation corridors
- Encourage pedestrian movement with improved walkability
- Maximise existing opportunities within the Town Centre
- Encourage provision of equitable access to vegetation and open space.



- Study Area Boundary
- Gateway
- Public Open Space
- Pedestrianised Mall / Potential Pedestrianised Zone
- Pedestrian Link / Laneway



Three Key Places are highlighted within the Town Centre for further investigation to provide a vision each public domain space.

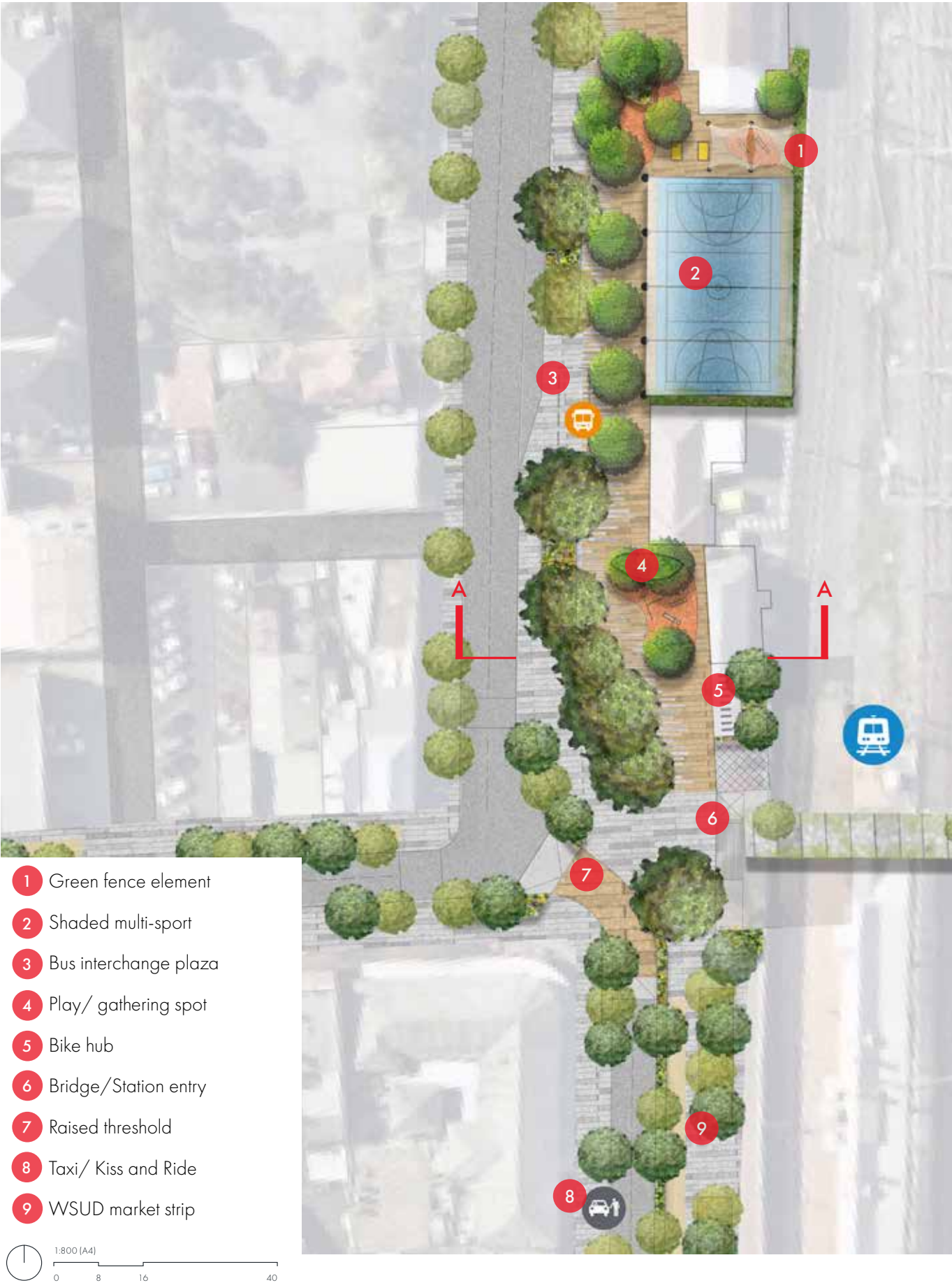
These are places in the heart of the town and are spaces which have numerous benefits for public domain improvements as part of open space network.

The Key Places:

- Are in locations that will increase provision and accessibility of open space within the Town Centre
- Respond to different context within the Town Centre, therefore have different character, programming and user requirements; and
- Provide a complimentary sequence of spaces which are significant as individual places as well as having a strong relationship together as part of a cohesive open space network.



5.2 Western Heritage Precinct : Jersey Street Park



## Jersey Street Park

A contemporary and active public domain space integrated with a new station entry, potential new railway corridor east west crossing and transport interchange.

A bright and vibrant place with a focus on youth programs, sports and play in a heritage civic setting.

Integrated into the rich heritage character with opportunities for creative approach to greened fence structures providing vantage points and sculptural elements to the space.

A space which will have broad appeal for a diverse range of active and passive users, which will have a change of energy levels across the day, week/ weekend and seasons.

Proximity of a transport interchange will encourage activation and support bus access to use the facilities, empowering youth and older people who may not have car access.



Top Left: Place Laboratory - Landezine  
 Top Right: The Tide - MTLURB  
 Middle Left: Box Hill - Pinterest  
 Middle Right: MFO Park - Pinterest  
 Bottom Left: Place Laboratory - Landezine  
 Bottom Right: RMIT - Indesignlive







### Links with Hornsby Quarry regional recreation destination

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On arrival at this precinct from the introduced east/ west crossing, and possible new train station entry point there are notable views west along Coronation Street, towards the quarry and existing bushland. The visual and physical connections with the quarry can be celebrated here. The focus on physical activity in the Jersey Lane public domain provides a complimentary space to the regional recreation activities to be provided in Hornsby Park



### Multi-sports courts

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This space includes a shaded court area which can be marked for a number of sport codes, a kick-a-bout space and table tennis tables. Location of this activity area is deliberately set within a passively supervised space, near the transport interchange. Inclusion of shady seating and arranged within proximity of a formal play space will encourage a diverse mix from the community to engage with the space.



### Play Areas

---

Two play spaces are considered in this precinct. One formal shaded play space within proximity of the multi-court spaces. There will be an opportunity for some programmable water play, play equipment for pre-school and primary school aged children and breakout opportunities into the nearby multi-sport courts space. An informal play space is also offered within the heritage courtyard where an interpretive play element which can be combined with a gathering/ seating space.



### Bus Interchange Plaza

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A generous shelter with ample seating and designed as a plaza gathering space allow this area to contribute to the Jersey Lane precinct presents an opportunity for this to be more than a bus stop and waiting area, and instead a valued community space.



### Green fences element

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Opportunity for a sculptural green wall boundary detail to the rail corridor. Vegetation would provide a useful screening/ protective fencing element as well as a dramatic backdrop for the multi purpose play space.



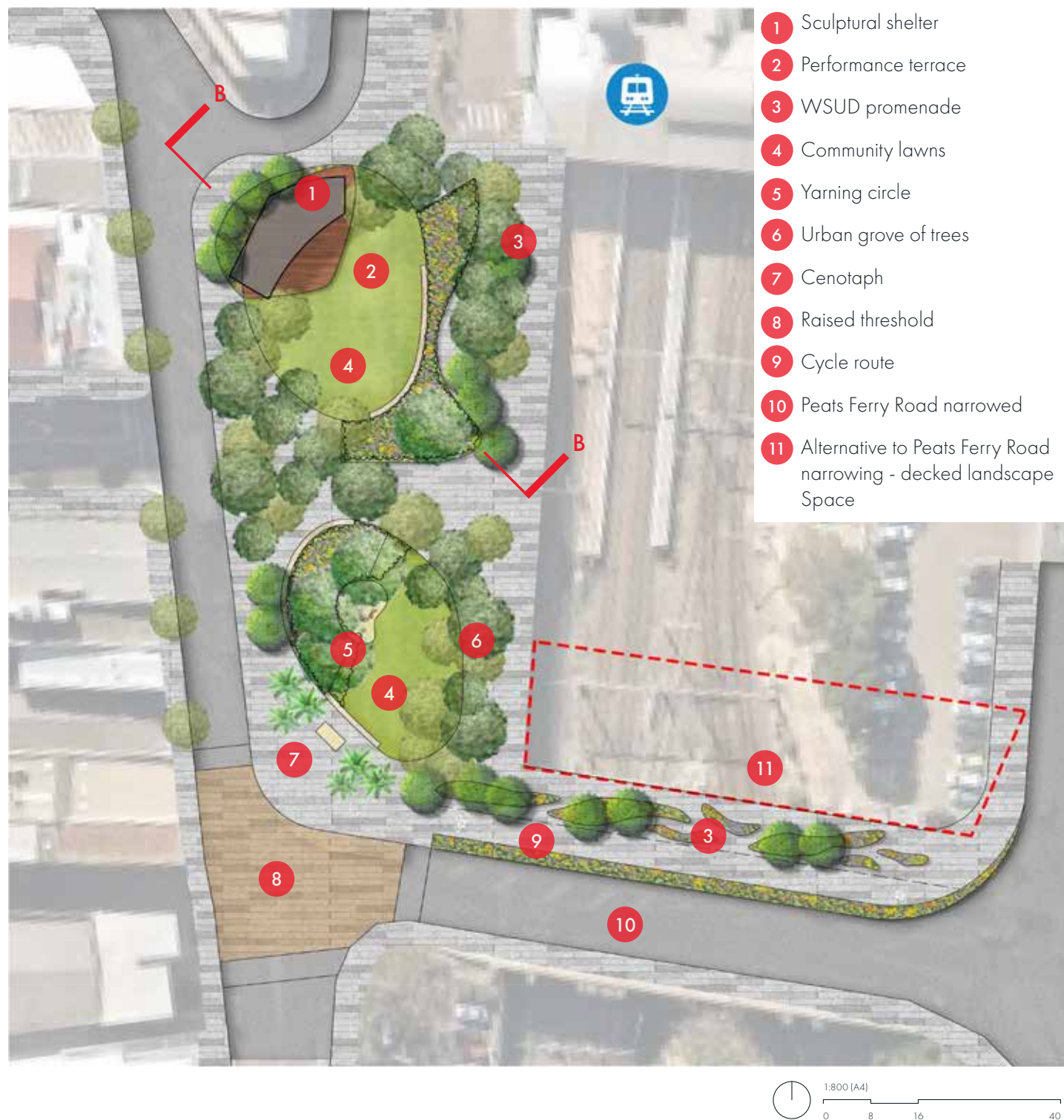
### Station Street Streetscapes

---

Station Street is designed with a focus on pedestrian amenity, with opportunities for community markets or coffee carts and social meeting thoroughfare. Removing the bus interchange provides space for pedestrian uses and increased street trees and vegetation. Including a Water Sensitive Urban Design option in this streetscape will demonstrate the integration of the ecological vision in the heart of the Town Centre.



### 5.3 Western Heritage Precinct: Cenotaph Plaza





## Cenotaph Plaza

A central park space which provides a unique Hornsby public domain environment.

A clearing in the forest, a space for informal meeting, a place for reflection and community gathering. A park with contemporary materials and functions while reflecting a natural history.

Responding to organic forms, an acknowledgment of concealed landforms and perhaps ancient stories told by stones rubbed and weathered. A permeable leafy, shady park.

Cenotaph and flags are relocated to a prominent location fronting Peats Ferry Road. With strong visual connections to the RSL, formal events can have the opportunity to spill into the road corridor and RSL frontage space.



Top Left: Place del Pi - Pinterest  
 Top Right: Chippendale Green - Turf  
 Middle Left: La Place Franco - Pinterest  
 Middle Right: Sammons Park - ATPAC  
 Bottom Left: Calgary Urban Park - Cision  
 Bottom Right: Macquarie Uni Campus - Macquarie University





## A Central Park in the Heart of Hornsby

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A opportunity to return a park space back to Town Centre. A substantial place which is green, restful and comfortable to. The space welcomes those arriving from the south and reinforces the character of Hornsby to be one that celebrates its bushland context.



## Movement and Access

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Movement to/ from the train station, particular along desire lines from major pedestrian road crossing points, flows through the central paved spaces and around the lawns. Consideration is given to ensure general pedestrian movement is not impacted during any organised events.



## Cenotaph Memorial space

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Cenotaph remains a central element in this park.

Relocated into a prominent position the memorial will feature as a gateway and landmark moment for the town centre with a strong visual connection to the RSL nearby. When in event mode, there is opportunity to take advantage of the whole southern space for formal occasions, with gathering space available around the memorial and flags.





## Community lawns

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Two reflective and passive lawn spaces with opportunity for informal gatherings. Providing two separately lawns offers flexibility for organised events and community programs. A companion green space to Hornsby Park and a contrast to Hornsby Square and commercial civic spaces.



## Performance space and Sculptural Shelter

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A decked covered area which will have multiple uses for entertainment both formal and informal. The shelter is located as a backdrop for the performance space as well as being a distinct visual element. Deliberately placed to provide a focal point for views south from Peats Ferry Road and as a pivot point on the corner of the park space.



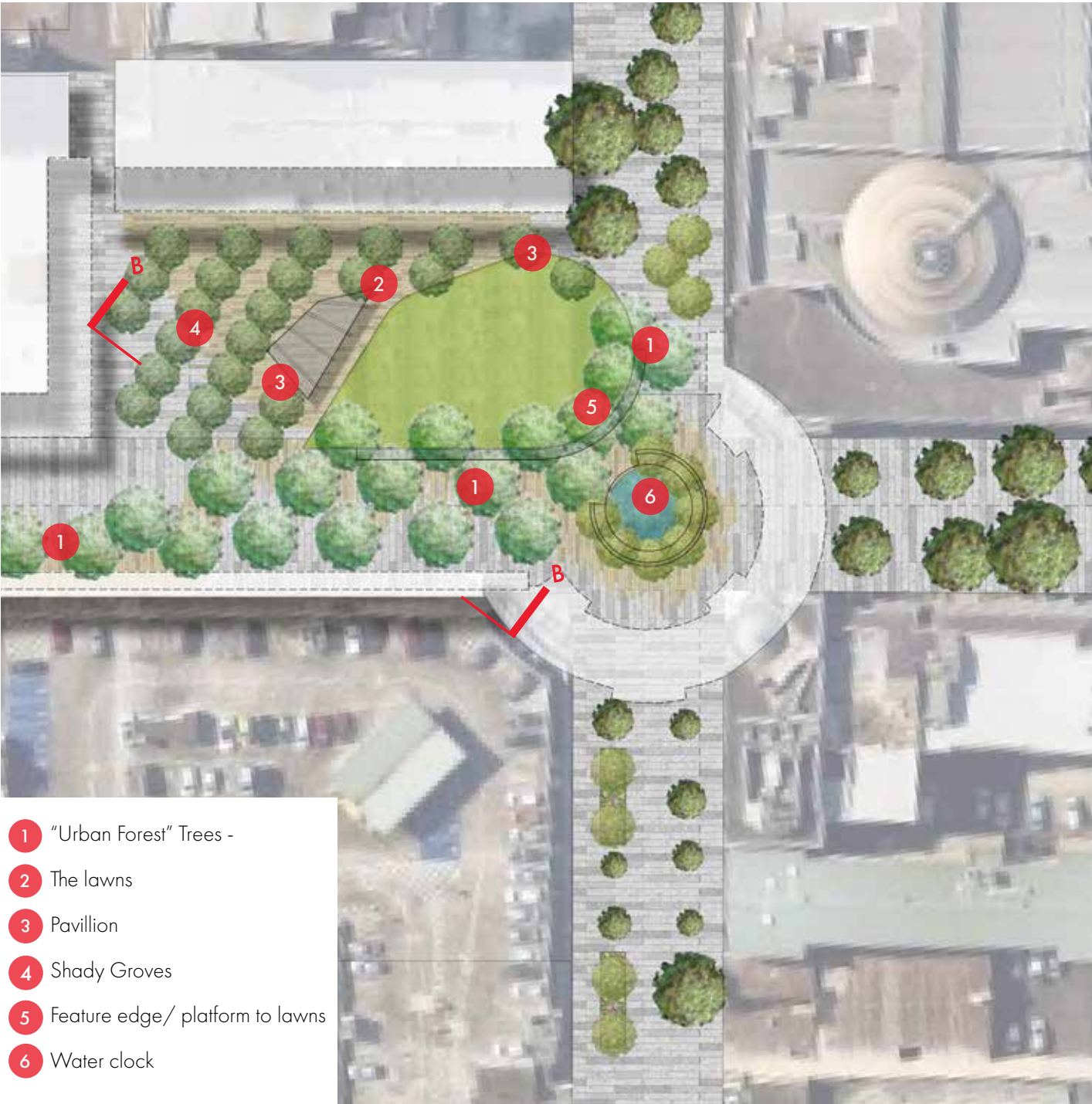
## Sheltered Yarning Circle

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A protected shady and sheltered space which invites a moment of gathering, staying, resting and sharing. A symbolic and central gesture towards acknowledging the benefits of respecting existing and fostering new local community connections.



5.4 Central Heart Precinct: Hornsby Square/ Florence Mall



## Hornsby Square and Mall

A lively urban plaza which provides a gathering and meeting place within the commercial Town Centre.

A central civic heart which plays a key role in the network of local pedestrian laneways and pocket spaces. The Square provides an important local open space for local residents and workers while providing an important connection within the broader network of neighbourhood open spaces for the whole Town Centre.

It is a space which enlivens the Town Centre and benefit the neighbouring laneways network. It provides a spill out for community civic buildings, outdoor eating for restaurants and café kiosks, quick meet ups, or opportunity for rest within the retail zone.

In this central public space, best practice Urban Forest and WSUD strategies can be displayed through innovative design. Introduction of trees with clever sub structural detailing to improve urban tree canopy experience, with deliberate use of stormwater and surface materials to celebrate and demonstrate WSUD techniques.



Top Left: David H. Koch Plaza-  
Pinterest  
Top Right: Darling Square,  
ArchDaily  
Middle Left: Darling Square,  
TRACT  
Middle Right: Darling Quarter,  
TRACT  
Bottom Left: MPavillion -  
ArchDaily  
Bottom Right: Chandler History  
Museum - DLR Group







## Movement and Access

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Within the Square, pedestrian desire lines are accounted and provided for, with generous provision around the square at shop fronts and intersection points. Spatial layout of the pavilion, lawn and shady grove allows for movement through the space without interrupting activities and use. Location of trees will accommodate vehicle operations and emergency access.

## Pavilion

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An architectural gem which creates a focus for the square. Can have multiple faces, with flexible roles. A shady kiosk within the grove of trees, a bandstand for entertainment, a screen for outdoor cinema, a stage for events, a community workshop space. It will be a visual highlight for the space from all angles and a night time sculptural lighting feature.

## The Lawn

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A local-scale turfed space, resting, for picnics, informal gathering, provides contrasting experience to the immediate commercial surroundings.

## Gathering Edge

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An architectural seating platform/ edge detail to form the raised lawn space. Provides an anchor for the street space while protecting and enclose the lawn, a magnet for informal gathering, meeting, resting and waiting.

## Urban Tree Canopies

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An opportunity to bring bush character right into the heart of Hornsby. Tall native trees provide a shade and wayfinding. Introduction of trees where ground conditions allow will encourage connection of local canopy into the broader urban forest network.



Artists Impression: Hornsby Square





### Shady Grove

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A grove of smaller scale trees create a spill out space for library/ community buildings. Provides a forecourt to the civic buildings and become a seamless interface with the plaza space.

### Hornsby Water Clock

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Retained in place in this design as a local icon at the meeting point of the pedestrian streets. Introduction of small trees, with opportunity for raised planters to create a vegetated edge refreshes this local landmark. With the focus of the space realigned towards the Pavilion and the Lawn, design of Hornsby Square allows for future options to be flexible and removal will have no negative impact on experience or enjoyment of the space.



Artists Impression: Hornsby Square

## 5.5 Central Heart Precinct: Burdett Street Park



- |  |                                 |
|--|---------------------------------|
| 1 Garden lawns                             | 7 Seating elements              |
| 2 Burdett Street share path                | 8 Shelters with outdoor setting |
| 3 Garden Picnic Grove                      | 9 WSUD opportunities            |
| 4 Spill out zone to building frontage      | 10 Garden feature/ story node   |
| 5 East/West footbridge overhead*           | 11 Laneway                      |
| 6 Flexi-use undercroft to ramps and steps* |                                 |



\* Bridge and ramp connections not designed.  
TBC during detailed design discussions between Council and proponent.



## Burdett Street Park

A town centre park space with a garden-like character that compliments the Hornsby Square and provides additional opportunities for passive recreation and informal gathering and meet the needs of a range of users. Generous paths, seating, plantings and shade provision will be critical to provide spill out spaces and places to dwell for larger gatherings, as well as more intimate gatherings or solo visitors.

The park will be large enough to accommodate high pedestrian flows and designed to act as a landing point for a pedestrian overpass linking the northern station concourse and Coronation Street. This would facilitate movement between the Hornsby east and west side and key anchors of the Hornsby Mall, Hornsby Park, Aquatic Centre and TAFE.

Flexible delivery of the park space includes options to provide upper level podium or ground level landscape spaces, or a combination of both, to add to canopy and greening, providing interest and leisure in outdoor settings.

Implementation of the park space will require considerate and thoughtful design to deliver a key access component of town centre upgrades by strengthening connection across the railway line, providing passive recreation and demonstrating environmental uplift in this highly urbanised setting.

Any basement or podium structure underneath is to be coordinated with requirements for connected deep soil plantings to support vegetation and mature tree growth, WSUD outcomes and reduced heat island effects.



Below: The Concourse  
Chatswood, Fjmt

Top Left: Westmead Hospital TRACT  
Top Right: Toorak Park, TRACT  
Middle Left: Berry Square, TRACT  
Middle Right: Berry Square, TRACT  
Bottom Left: Feature tree grates,  
IronAgeDesigns  
Bottom Right: MFO Park Zurich,  
Maschinenfabrik Oerlikon









## 5.6

### PUBLIC DOMAIN KEY PLACES & ELEMENTS

### STREETSCAPE DESIGN GUIDES

## 5.6 Streetscape Design Guides

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### 5.6.1 Overview and Implementation

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This section provides a summary of town centre street typologies with a clear and consistent direction for developers and stakeholders regarding the design, arrangement and materiality of the public realm within Hornsby Town Centre.

The intent is to make the streetscapes more consistent and legible, visually pleasing and well integrated.

#### Streetscape Design Principles:

- Be simple in design, consistent in material and economical in construction;
- Have clarity: The structure of the street should be clear, balanced and equitable;
- Be consistent: Patterns and colours should be visually unifying and complementary. Materials to be durable and relevant to the local character. Selections build upon existing Council pallets and acknowledge opportunity for more bespoke arrangements in gathering and plaza spaces.
- Be economical: Be easy to construct, replaced and maintained.

The streetscape design guidelines will be used by Council using a variety of implementation strategies:

- **Complement overarching Council Policies and Strategies:** Documents such as Hornsby Public Domain Guidelines, Health Ageing Hornsby, Social Inclusion Hornsby and Walking and Cycling Strategy establish clear directions for the delivery of high quality public spaces that are safe, accessible and welcoming for all. Hornsby Town Centre spaces will consider and implement overarching directions and actions, in partnership with the Hornsby Town Centre Public Domain Guidelines.

- **Requirements through planning instruments, guidelines and control plans:** Public Domain concepts and guidelines will be integrated into all Council planning instruments to ensure these form part of any development approval mechanisms. This will guide future developments and outline Council expectations for new developments for the appropriate inclusion of high quality public domain.
- **Planning and advance coordination of services and utilities:** Advanced planning of all future utilities and services needs is coordinated to provide a cohesive approach to provision of new services for developments. This will capitalise on opportunities to integrate service improvements which align with public domain proposals particularly with coordination of undergrounding of overhead power and design of stormwater/ WSUD best practice.
- **Use Streetscape Guidelines as template for all new works:** Where public domain works are undertaken, use the materials and furniture palettes to guide all future development at all scales, allowing Hornsby identity to consolidate with smaller short term projects, while larger projects evolve.
- **Dementia Friendly Design Considerations:** Dementia friendly design goes beyond the application of inclusive access applications and Australian Standards to provide functional, legible and safe environments for all abilities.
  - Provide acoustic barriers to reduce background noise, such as planting and fencing.
  - Use clear large signage with clear colour contrast for wayfinding to toilets and destinations.
- Provide obvious and easy access to shaded areas, with adequate seating.
- Provide seating design which is clearly a seat, avoid abstract furniture design.
- Provide sufficient parking and easy access to parks, shops and public facilities.
- Ensure changes in ground levels are clearly marked and well-lit, with compliant handrails, tactile, and appropriate contrast and non-glare surfaces.
- Ensure clear visibility from top step to bottom step.
- Provide sensory stimulation in gardens and parks using varied colours, scents, textures and sounds.
- Ensure main paths of travel are enough for two users to walk side-by-side or use wheelchairs.
- Ensure entry and exit signs are clear and obvious.
- Avoid structures, such as pergolas, or lighting which cast complex contrasting shadows, these may be interpreted as depth changes.
- Ensure undercover areas are provided in parklands for access in all weather conditions.
- Utilise visual landmarks to assist with wayfinding such as water features or murals.
- Ensure bus shelters are enclosed and have adequate seating.
- Refer to Alzheimer's Australia's "Guidelines for the Development of Friendly Communities" and Local Government Toolkit.
- **Dementia Friendly Maintenance Considerations:** Maintenance of the public domain is equally as important as its design. The public domain infrastructure must be actively monitored and rectification of issue sought as soon as possible.



- Repair trip hazards caused by cracks, tree root up-heaving, unstable pavers, potholes and other depressions.
- Repair or replace damaged infrastructure.
- Repainting of surfaces, and line marking to maintain compliant luminance contrast.
- Cleaning of surfaces to removing debris and spills which may cause slip hazards.
- Tree and plant pruning and maintenance to prevent obstructions to a continuous path of travel, restricting of lighting or access to building entries and exits.

Integrated Street Design should be:

- Balanced to allow for equitable movement
- Designed to encourage healthy and active transportation modes of walking and cycling
- Ample with the provision generous footpaths
- Composed of durable, quality and aesthetically pleasing street furniture
- Encompassing of street trees, garden beds elements and WSUD best practice.

### 5.6.2 Integrated Street Design

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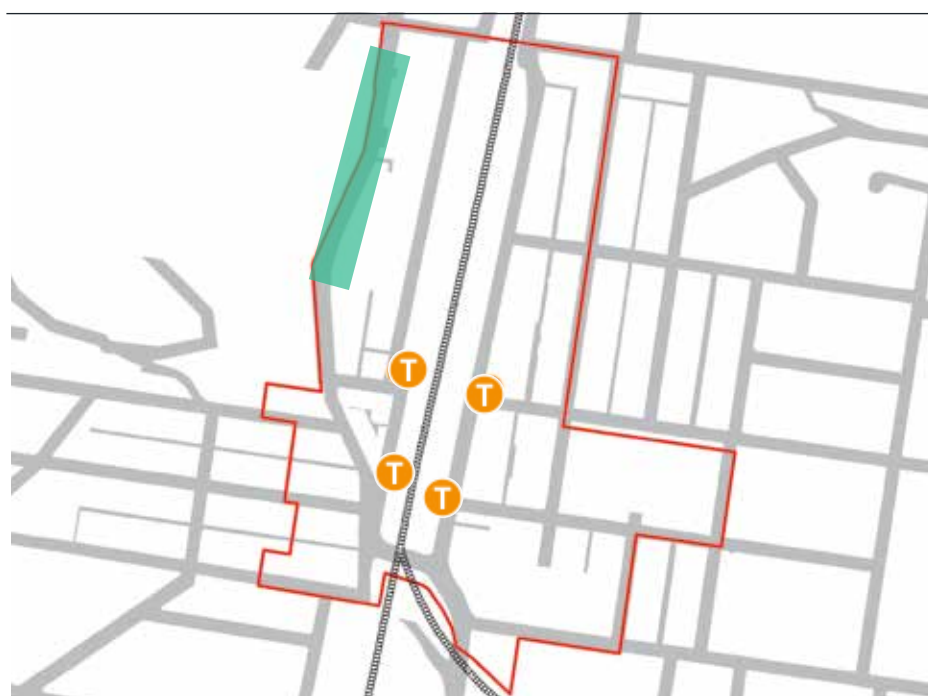
This is an opportunity to integrate multi-user needs into the street design and reduce vehicle dominated character within the town centre.

Traffic geometry and operational requirements to be integrated into a streetscape which favours pedestrian and cycle movement.








Streets also respond to the Structure Plan Precinct context and functionality with a hierarchy of street typology network. Each network serves a different function based on traffic, cycle and pedestrian circulation and the activities which occur within each Precinct. For example:

- George Street will continue to be a regional road link through the town, however streetscape verge design can ensure Hornsby Town Centre is acknowledged within this road corridor;
- Peats Ferry Road is suggested to accommodate local traffic allowing streetscape designs to favour provision for pedestrians and cyclists; and
- Within the Central Precinct street designs favour high integration of pedestrian movement and gathering activity.

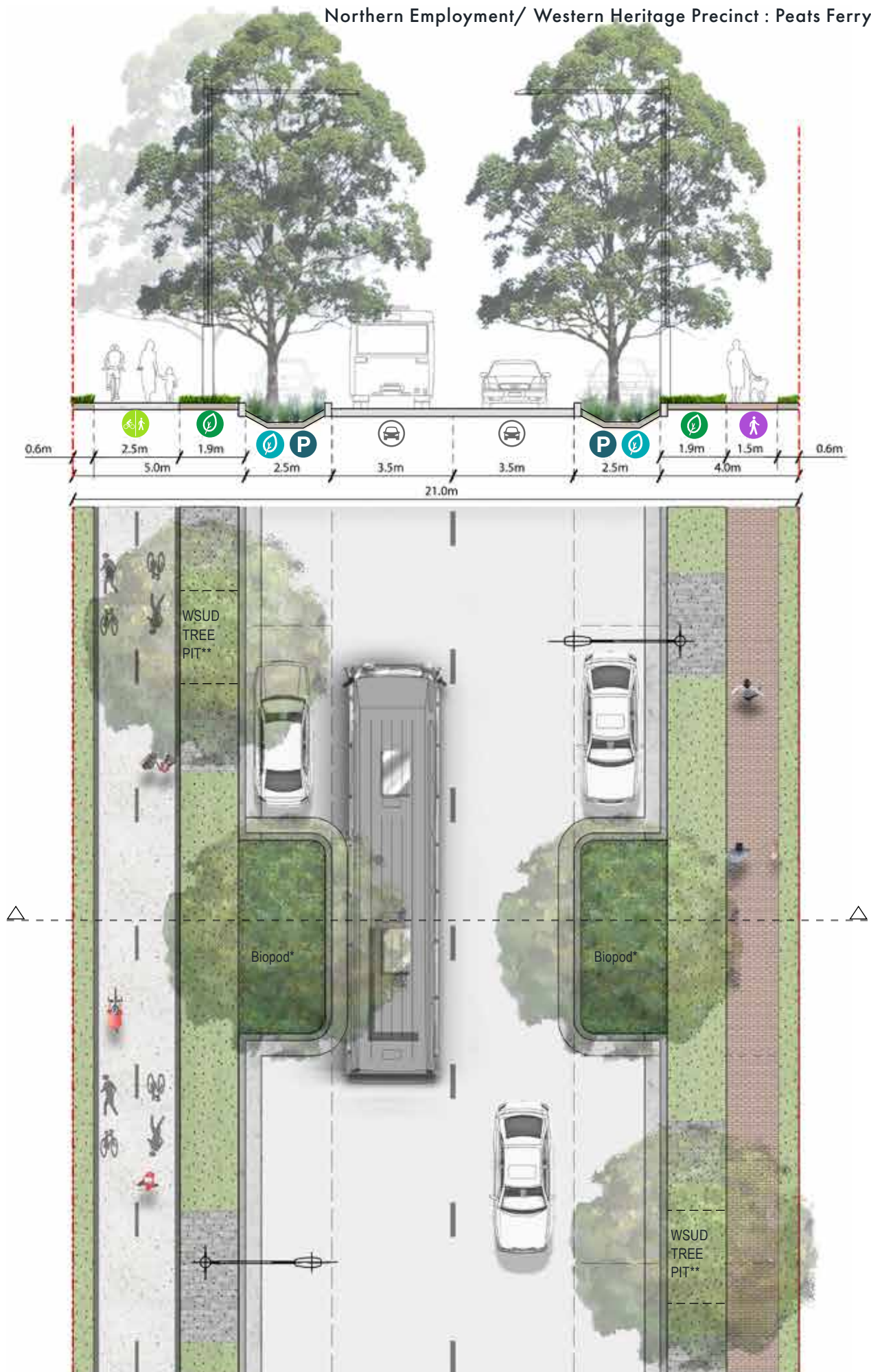
### 5.6.3 Northern Employment/ Western Heritage Precinct : Peats Ferry Road



#### Legend

-  Planting
-  Footpath
-  Shared Path
-  WSUD Features
-  Parking
-  Carriageway
-  Cadastre Boundary

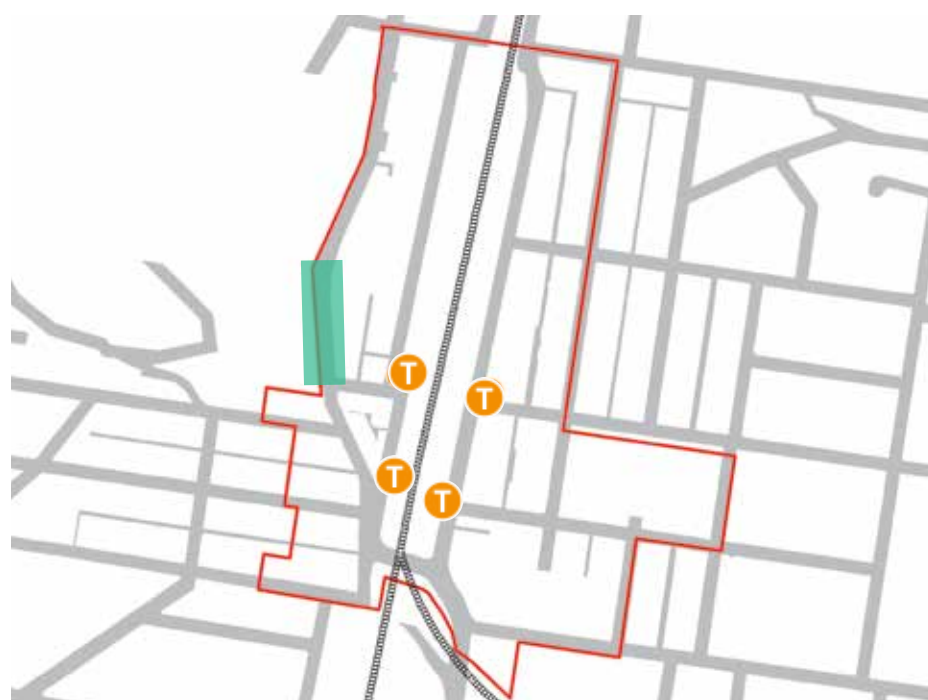
Key Public Domain Elements	Description
Street Type	Collector Road: HTC local adaptation
	Medium Zone 30km/h.
Location	Peats Ferry Road/Northern employment Precinct
Street Geometry	20m road reserve
	Shared cycle/ pedestrian path along west side.
	Footpath: 1.5m minimum allowance
	Parking: 2.5m wide/ parallel retain existing kerb alignment
	Traffic lanes: 3.5m wide/ retain existing kerb alignment
	Trees planted in verges and incorporated into delineated parking lane
Streetscape Elements	Traditional kerb and gutter treatment, slotted kerbs introduced where WSUD and stormwater opportunity to maximise surface water run off. Porous paving provides pedestrian access.
	Brick paved footpaths
	Street furniture: litter bins and wayfinding signage
	Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	Grass verge with street tree planting
	Street trees - tree trench root zone to extend under paving with engineered root cells/ structural support detail to ensure effective root zones available.
	Parking lane biopod: 4m x 2.5m min with extended structural soil trenching 8.6 m minimum length to ensure effective root zones available.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off. * Biopods 4 x 2.5m in parking lanes at ~80m centres. Location to align with street drainage. ** Or tree pits 2m <sup>2</sup> (minimum) at 10-15m spacings in verge zone as an alternative to Biopods at Council discretion










Concept Only. Final design subject to technical design considerations, topography, detailed investigation of services and relevant approvals



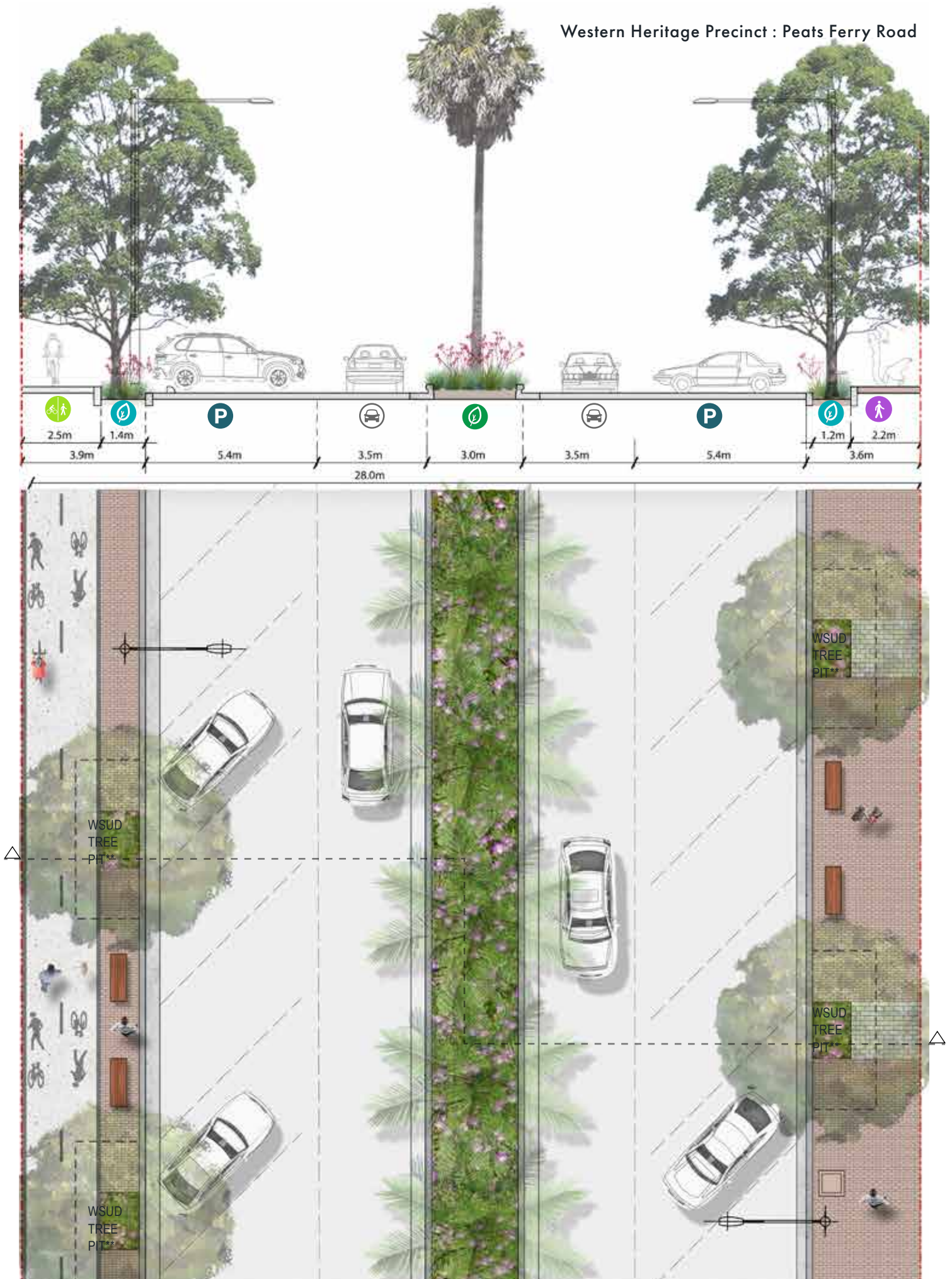
### 5.6.4 Western Heritage Precinct : Peats Ferry Road



#### Legend

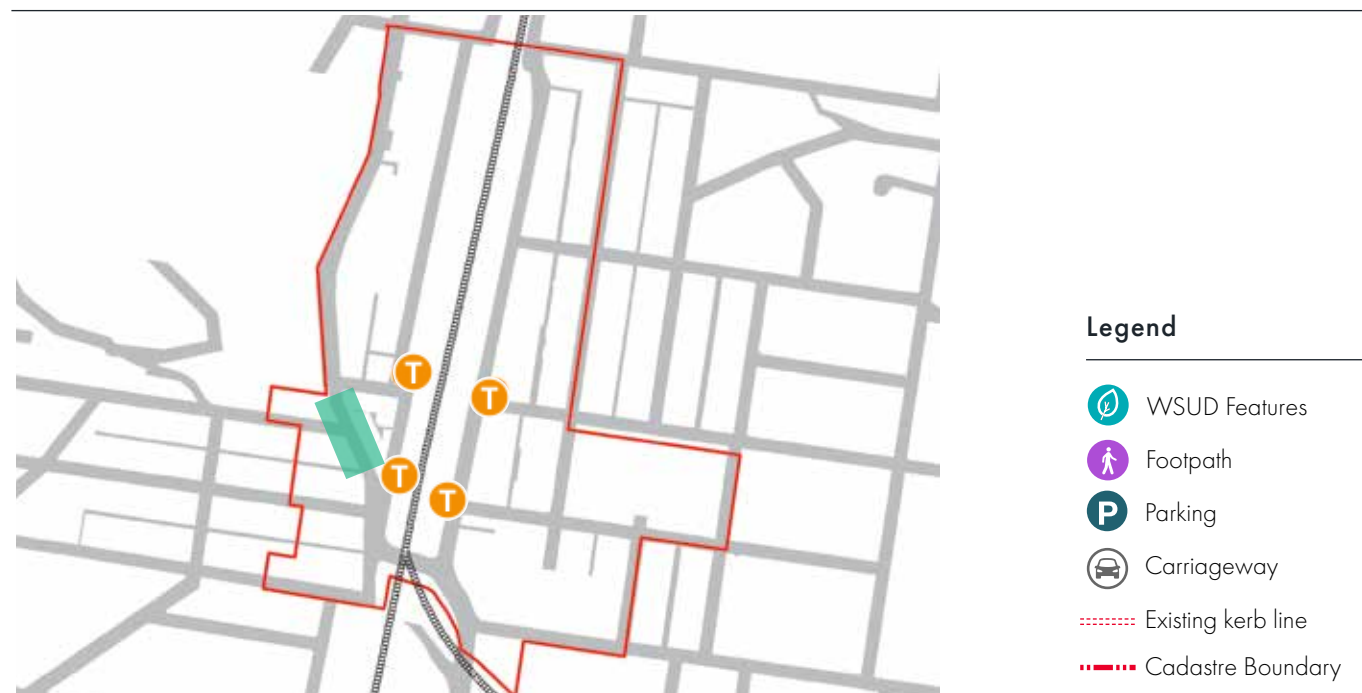
-  Planting
-  WSUD Features
-  Footpath
-  Shared Path
-  Parking
-  Carriageway
-  Cadastre Boundary

Key Public Domain Elements	Description
Street Type	Collector Road: HTC local adaptation Medium Zone 30km/h.
Location	Peats Ferry Road/ Western Heritage Precinct
Street Geometry	28m road reserve (Approx) including retained existing median planting Shared cycle/ pedestrian path along west side. Footpath: 3.6m wide Traffic lanes: 3.5m Parking: 2.5m wide bays angled layout (existing)
Streetscape Elements	Traditional kerb and gutter treatment, slotted kerbs where WSUD and stormwater opportunity to maximise surface water run off. Brick paved footpaths Multi function poles: street lights with banner and pedestrian lights Street Furniture: amenities located along the street for pedestrian comfort, in association with civic uses nearby
Landscape Treatments	Planted garden bed/ street tree pit. Tree pit: 1.5m min surface width, 4m x 2.5m min with engineered root cells extended structural soil trenching 8.6 m minimum length under hard surfaces for all new trees in paving, to ensure effective root zones available. Existing central median planting and trees retained and protected.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location. WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off. **Tree pits 2m2 (minimum) at 10cm centres in verge.



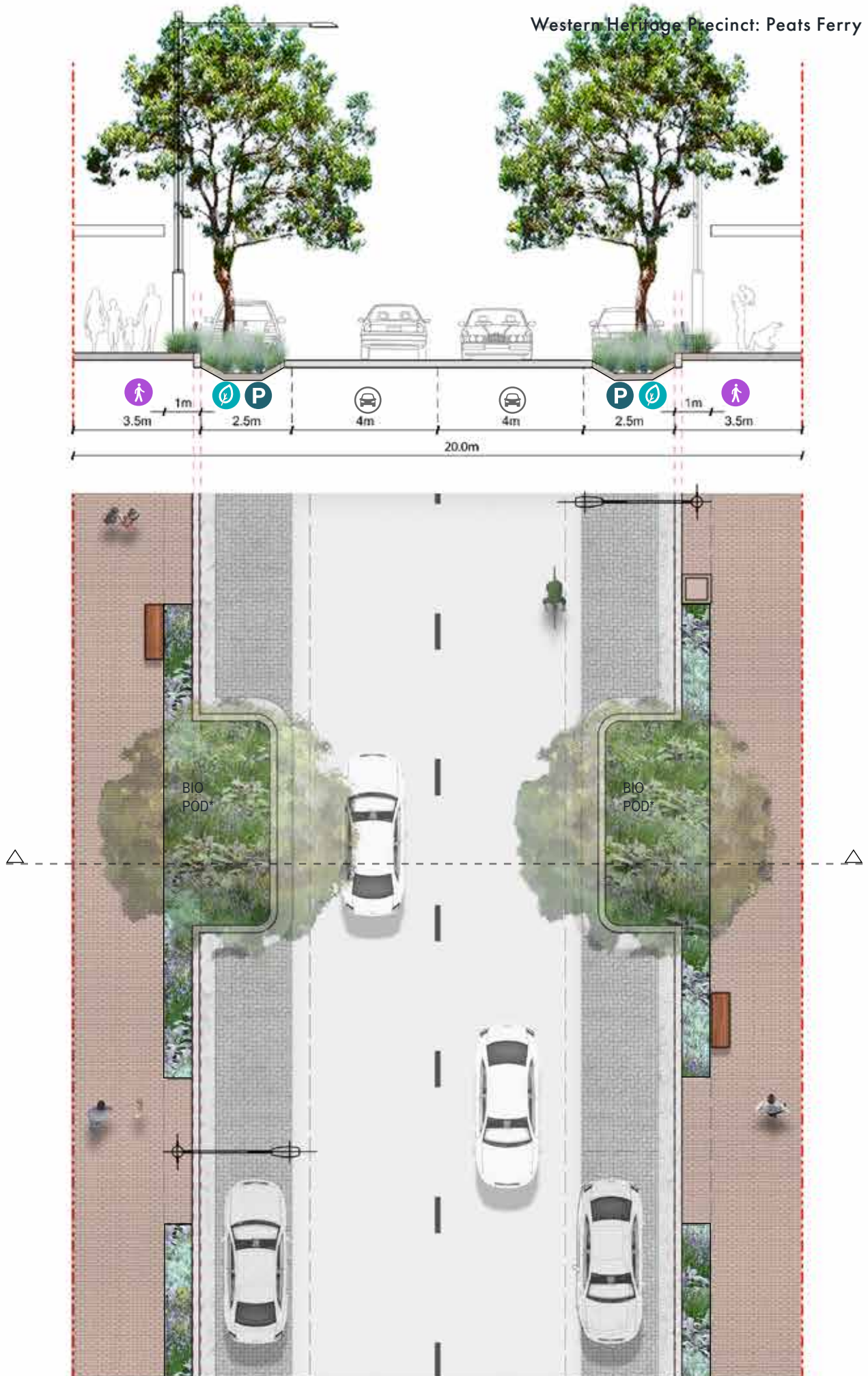
Concept Only. Final design subject to technical design considerations, topography, detailed investigation of services and relevant approvals

### 5.6.5 Western Heritage Precinct: Peats Ferry Road



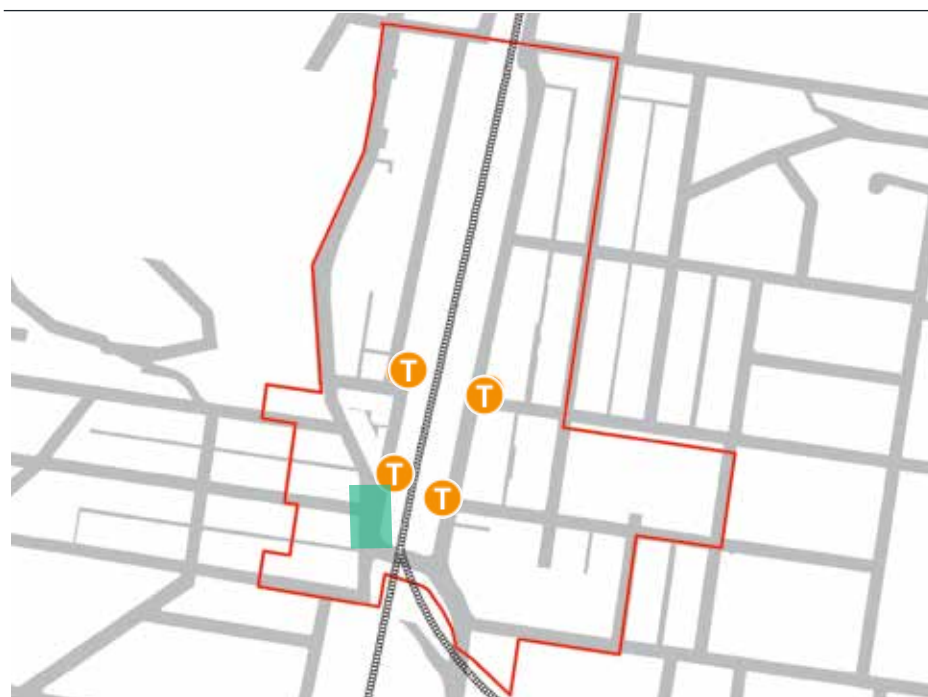
Key Public Domain Elements	Description
Street Type	Collector Road: HTC local adaptation
	Medium Zone 30km/h.
Location	Peats Ferry Road/ Western Heritage Precinct
Street Geometry	20m road reserve
	Footpath: 3.5 m wide verge (Existing)
	Parking: 2.5m wide/ parallel retain existing kerb alignment
	Traffic lanes: 4.0m wide (existing)
Streetscape Elements	Traditional kerb and gutter treatment, slotted kerbs where WSUD and stormwater opportunity to maximise surface water run off. Existing sub base engineered root cell structural system remains in place.
	Brick paved footpaths
	Brick paved parking lanes, porous jointing to be considered.
	Street furniture: seats, litter bins, bike racks and wayfinding signage, amenities located along street for pedestrian comfort
	Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	Existing planted garden beds with street tree planting, supplemented where appropriate.
	New tree pit: 1.2m min surface width, 4m x 2.5m min with engineered root cells/ extended structural soil trenching 8.6 m minimum length under hard surfaces for all new trees in paving, to ensure effective root zones available
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	Existing engineered structural system under parking bays retained.
	WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off towards existing trees.
	*Biopods 2.5 x 4m at ~80m spacings in parking lane where new street trees installed. Locations to align with street drainage.












Concept Only. Final design subject to technical design considerations, topography, detailed investigation of services and relevant approvals

### 5.6.6 Western Heritage Precinct: Peats Ferry Road

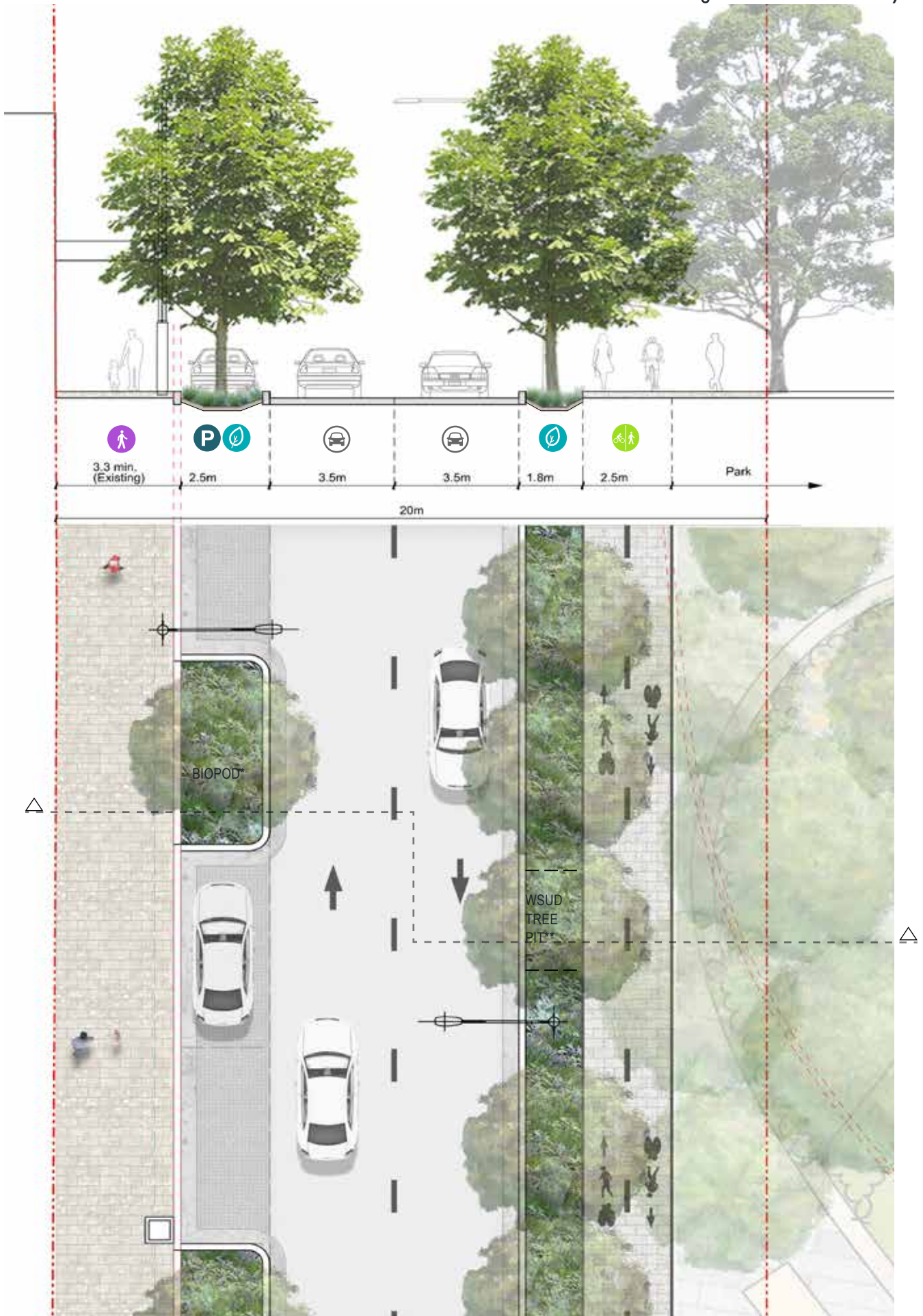


#### Legend

-  Shared Path
-  Footpath
-  Carriageway
-  Parking
-  WSUD Features
-  Cadastre Boundary
-  Existing kerb line

Key Public Domain Elements	Description
Street Type	Collector Road: HTC local adaption
	Medium Zone 30km/h.
Location	Peats Ferry Road
Street Geometry	20m road reserve
	Shared cycle/ pedestrian path along east side.
	Footpath: 3.3m min wide
	Parking: 2.5m wide/ parallel retain existing kerb alignent
	Traffic lanes: 3.5m wide
Streetscape Elements	Traditional kerb and gutter treatment, slotted kerbs where WSUD and stormwater opportunity to maximise surface water run off.
	Brick paved footpaths/ shared path materials at interface with park to be matched as appropriate.
	Brick paved parking lanes, porous paving to be considered.
	Street furniture: seats, litter bins, bike racks and wayfinding signage, amenities located along street for pedestrian comfort
	Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	Planted garden beds with street tree planting.
	New tree pit: 1.2m min surface width, 4m x 2.5m min with engineered root cells/ extended structural soil trenching 8.6 m minimum length under hard surfaces for all new trees in paving, to ensure effective root zones available
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off towards existing trees. * Biopods 2.5 x 4m in parking lane at 80m spacings. Location to align with street drainage. * * Or tree pits 2m <sup>2</sup> (minimum) at 15m centres in verge as an alternative to Biopods at Council discretion.

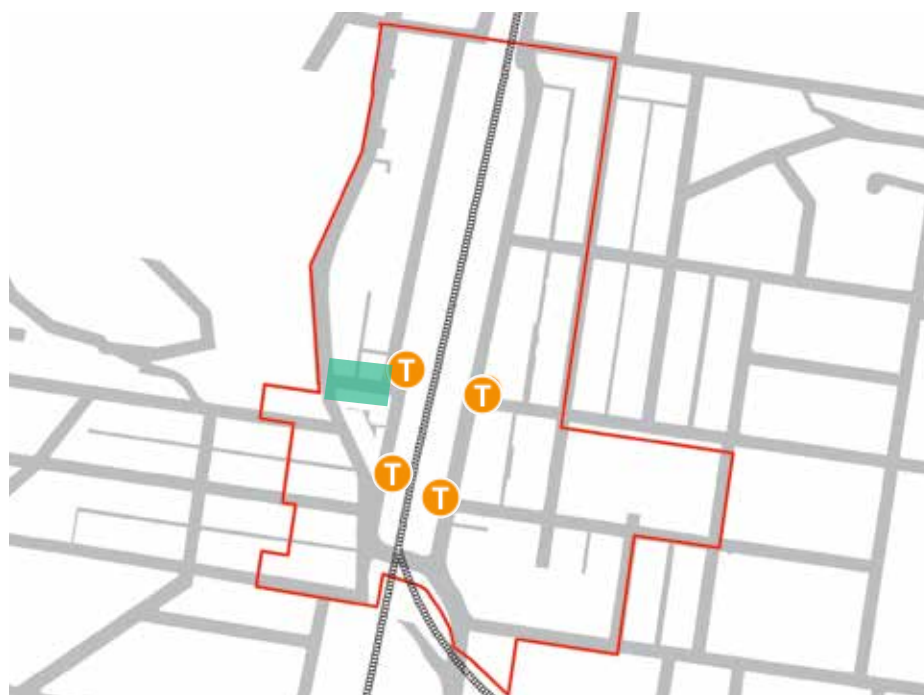











Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
 Concept Only. Final design subject to technical design considerations, topography,  
 detailed investigation of services and relevant approvals



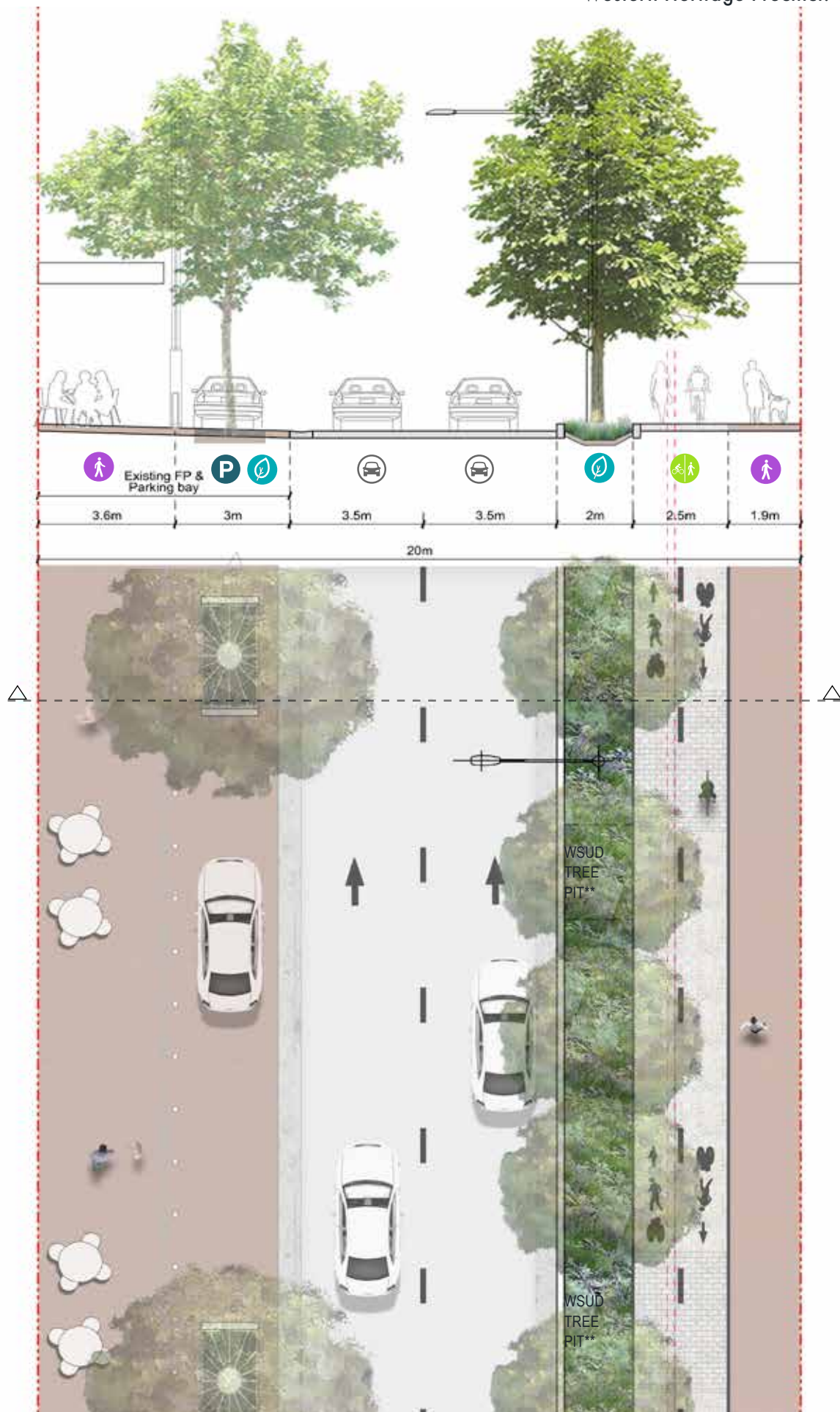
### 5.6.7 Western Heritage Precinct: Coronation St



#### Legend

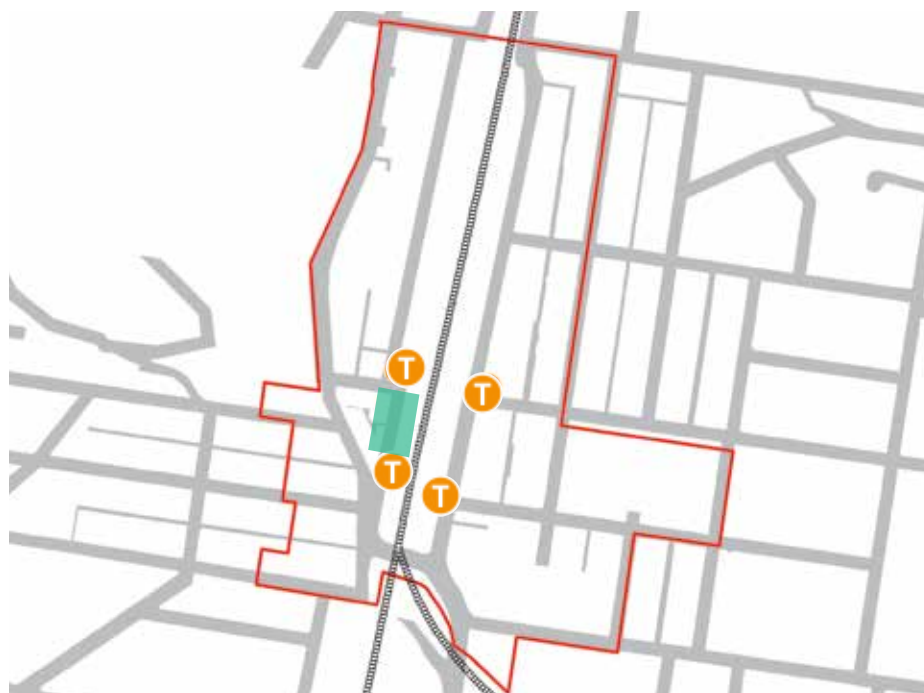
-  Shared Path
-  Footpath
-  Carriageway
-  Parking
-  WSUD Features
-  Cadastral Boundary
-  Existing kerb line

Key Public Domain Elements	Description
Street Type/ Character	Local Street/ HTC local adaptation
	Medium Zone 30km/h
	Interface between Precincts
Location	Coronation Street/ Central Heart Precinct
Street Geometry	20m road reserve
	Footpath: 1.5m wide to verge at residential interface
	Parking: 2.5m wide parallel parking
	Traffic lanes: 3.5m wide, one way
Streetscape Elements	Footpath: Brick paving.
	Share Path: Material generally, insitu concrete pathways with brick detail to assist with delineation of Share Path through high activity area. Opportunity shown for dedicated shared path. Consideration for signage to manage cycle behavior TBC.
	Street furniture: seats, litter bins, bike racks and wayfinding signage. Amenities located along street for pedestrian comfort
	Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	Continuous planting bed to west side. Street trees and ground covers
	Street Tree Planting- for all new trees in paving. Tree pit to be 4m x 2.5m min with engineered root cells/ structural support under hard surfaces.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off towards existing trees. **Tree pits 2m <sup>2</sup> (minimum) at 15m centres in verge.












Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
 Concept Only. Final design subject to technical design considerations, topography,  
 detailed investigation of services and relevant approvals

### 5.6.8 Western Heritage Precinct: Station St

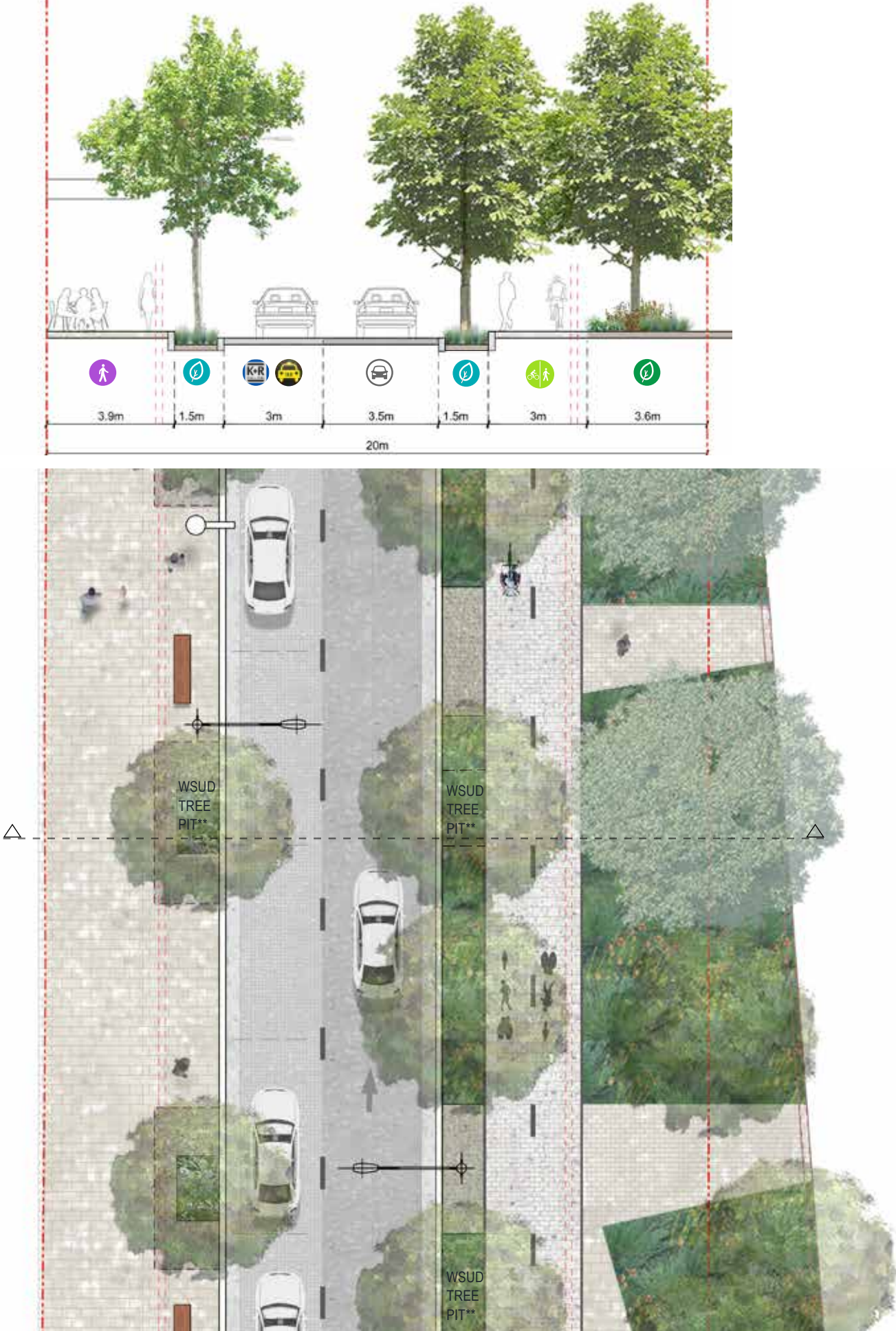


#### Legend

-  Planting
-  WSUD Features
-  Shared Path
-  Footpath
-  Carriageway
-  TAXI Rank Lane
-  Kiss&Ride Lane
-  Cadastre Boundary
-  Existing kerb line

Key Public Domain Elements	Description
Street Type/ Character	Share Street
	Slow Zone 25km/h
Location	Station Street/ Central Heart Precinct
Street Geometry	20m road reserve
	Sharepath: 3.0m wide to west side of street. Opportunity shown for dedicated shared path. Consideration for signage to manage cycle behavior TBC.
	Taxi & Kiss & Ride lane: 2.5m wide parallel parking
	Traffic lanes: 3.5m wide, one way
Streetscape Elements	Carriageway: Trafficable brick paved surface.
	Footpath: Brick paving
	Share Path: Material generally, brick with further detail to assist with delineation of Share Path through high activity area.
	Street furniture: seats, litter bins, bike racks and wayfinding signage. Amenities located along street for pedestrian comfort
	Lighting poles: street lights/ pedestrian lights
Landscape Treatments	Street trees in paving to have trench root zone extend under paving with structural support detail to ensure effective root zones available.
	Street Tree Planting- for all new trees in paving. Tree pit to be 4m x 2.5m min with engineered root cells/ structural support under hard surfaces.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	WSUD features have opportunities for: Slotted and flush kerbs as appropriate to maximise collection of surface water run-off towards existing trees. **Tree pits 2m <sup>2</sup> (minimum) at 15m centres in verge





Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
Concept Only. Final design subject to technical design considerations, topography,  
detailed investigation of services and relevant approvals

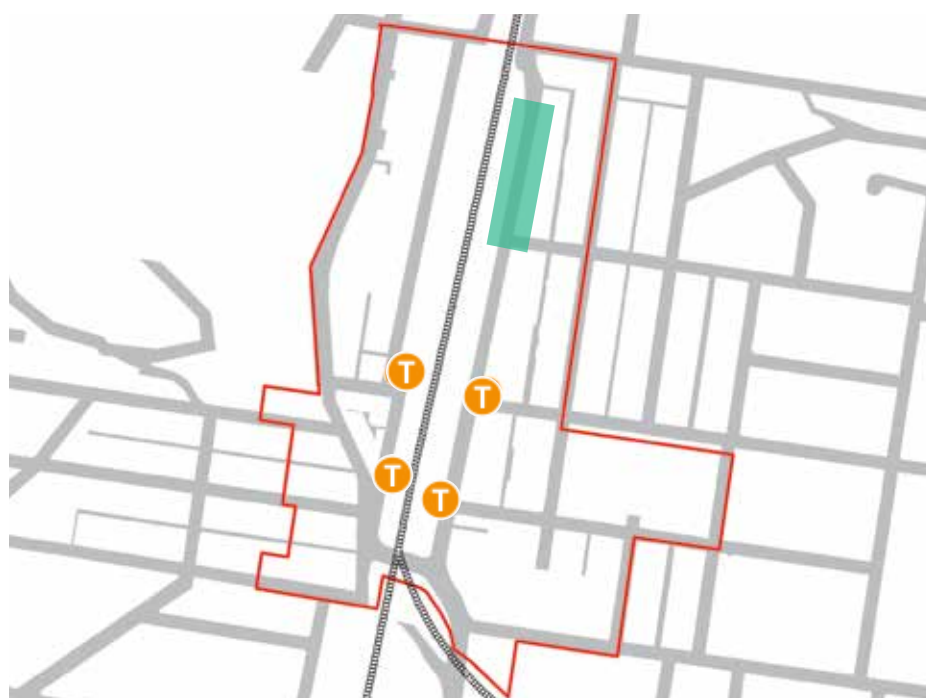
## Western Heritage Precinct: Station St








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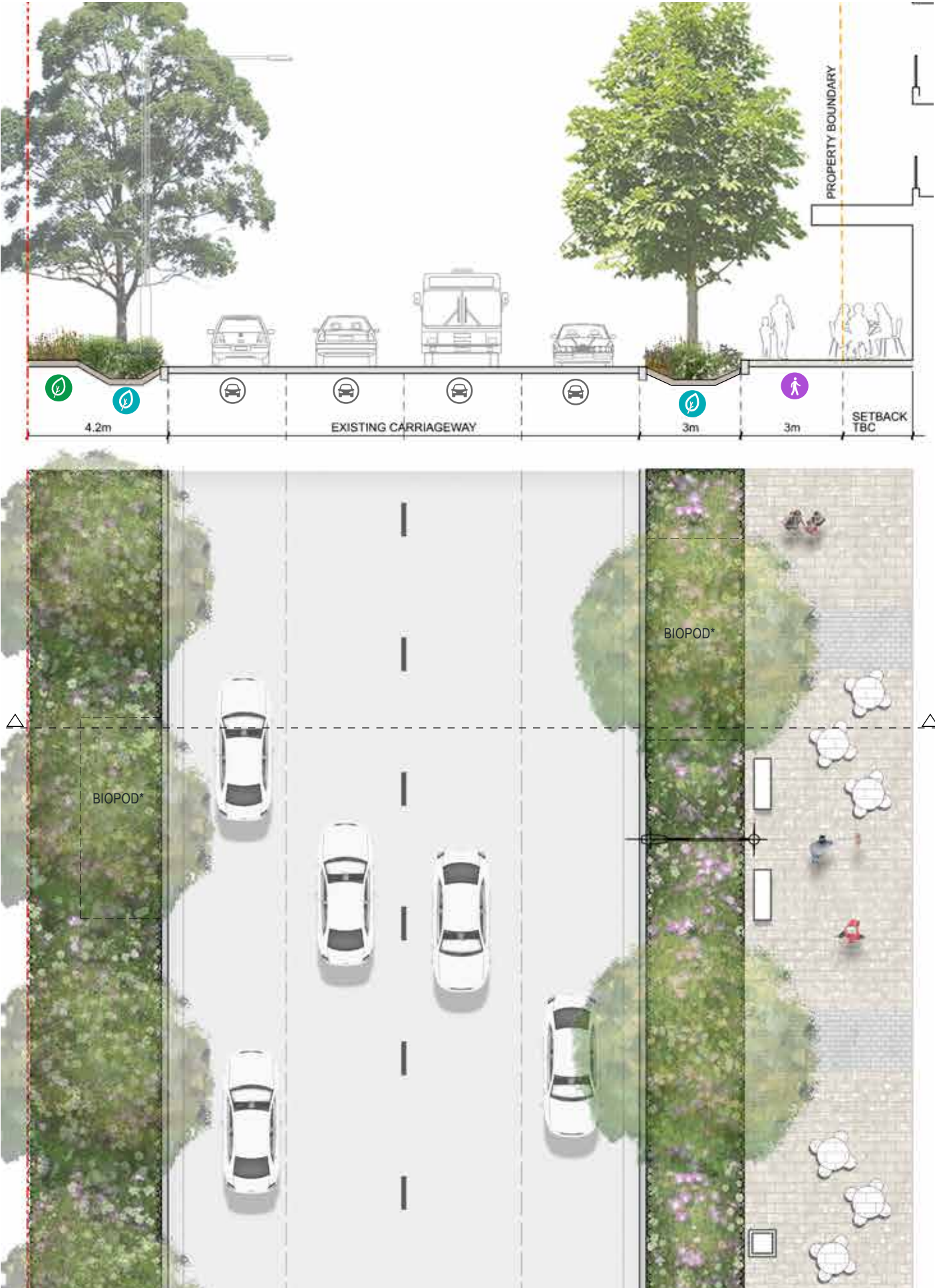
### 5.6.9 Northern Employment Precinct: George Street



#### Legend

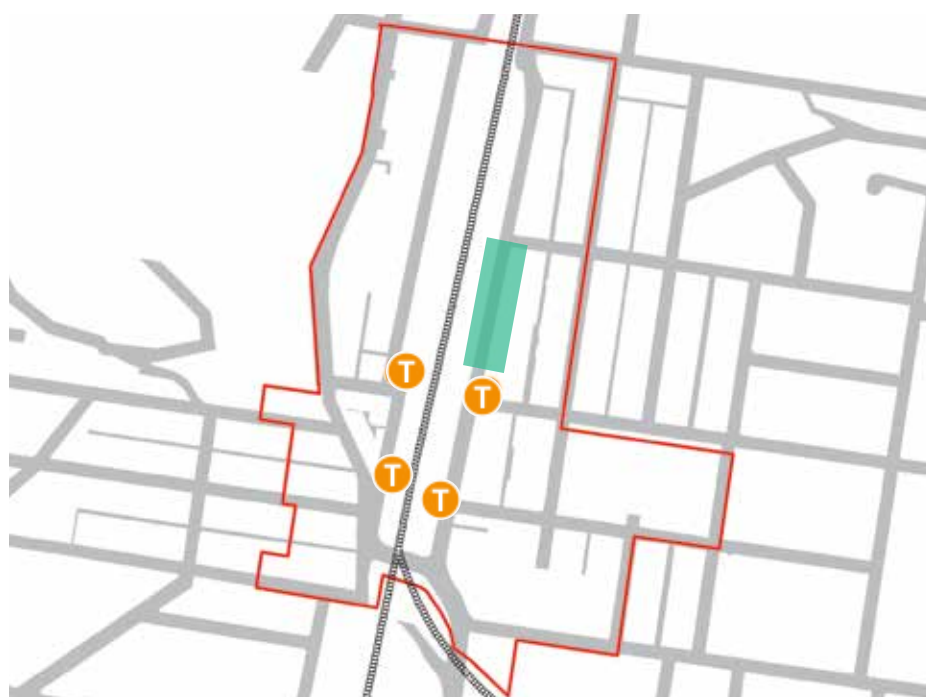
-  Planting
-  Footpath
-  Carriageway
-  WSUD Features
-  Cadastre Boundary

Key Public Domain Elements	Description
Street Type/ Character	Arterial Road
	Medium Zone 50km/h.
Location	George Street/ Northern Employment Precinct
Street Geometry	Existing road reserve
	Footpath: 3.0m wide plus agreed setback to building frontage.
	Traffic lanes: existing
	No parking
Streetscape Elements	High quality precast paving footpath material generally, with feature paving in focus areas.
	Street furniture: seats, litter bins, bike racks and wayfinding signage. amenities located along street for pedestrian comfort, in association with uses near by.
	Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	Continuous planted verge under street trees.
	Trees to have continuous verge root zone. Where hard paved surface, not garden bed, engineered root cells/ structural soil support to be used - 4m x 2.5m min extended beyond tree pit.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones
	WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off towards existing trees. *Verge biopods 2.5 x 4m at 70m spacing. Locations to align with street drainage. Only applicable on southern end of street.







Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
Concept Only. Final design subject to technical design considerations, topography,  
detailed investigation of services and relevant approvals

## 5.6.10 Central Heart Precinct : George St - Alternative

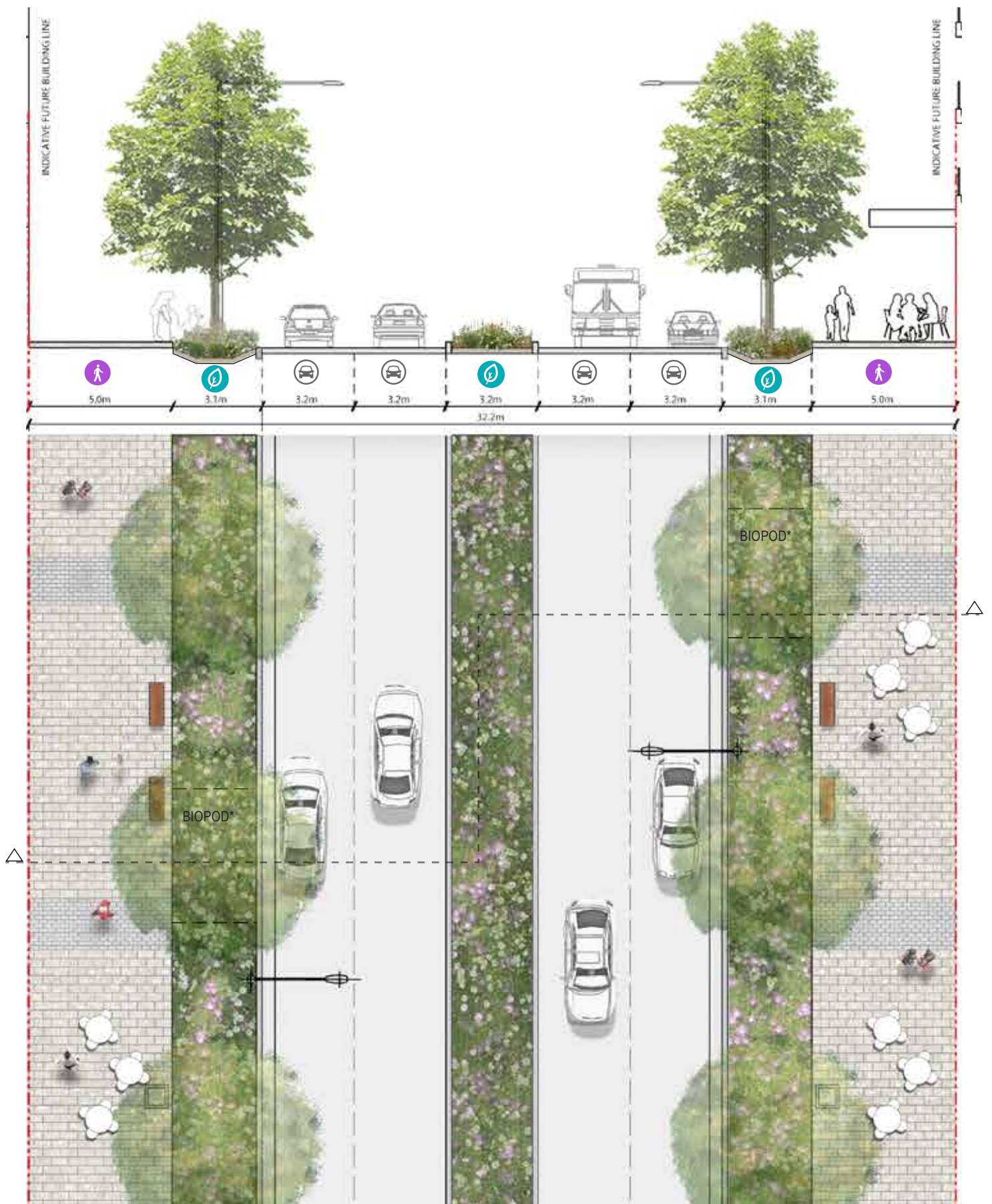


### Legend

-  Footpath
-  Carriageway
-  WSUD Features
-  Cadastre Boundary

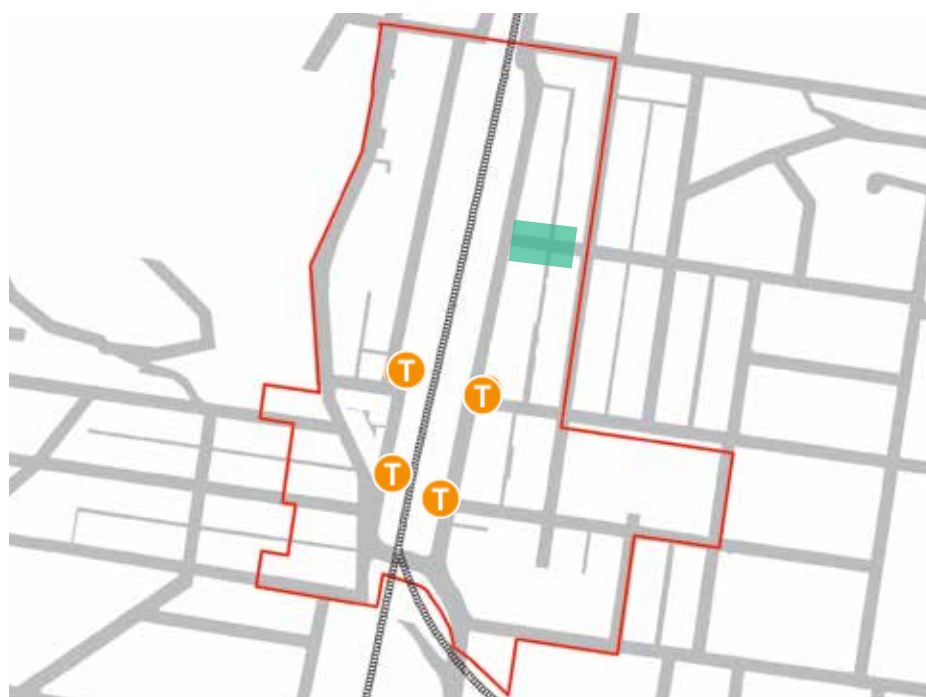
Key Public Domain Elements	Description
Street Type/ Character	Arterial Road - HTC local adaptation Medium Zone 50km/h.
Location	George Street - Central Heart Precinct Alternative
Street Geometry	Existing road reserve Footpath: 3.0m wide plus agreed setback to building frontages to both sides Traffic lanes: existing No parking
Streetscape Elements	High quality precast paving footpath material generally, with feature paving in focus areas. Street furniture: seats, litter bins, bike racks and wayfinding signage. Amenities located along street for pedestrian comfort, in association with uses near by Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	Continuous planted verge under street trees. Trees to have continuous verge root zone. Where hard paved surface, not garden bed, engineered root cells/ structural soil support to be used - 4m x 2.5m min extended beyond tree pit.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location. WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones. WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off towards existing trees. *Verge biopods 2.5 x 4m at 50m spacings. Location to align with street drainage.











Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
 Concept Only. Final design subject to technical design considerations, topography,  
 detailed investigation of services and relevant approvals

### 5.6.11 Central Heart/ Northern Employment Precinct : Linda St

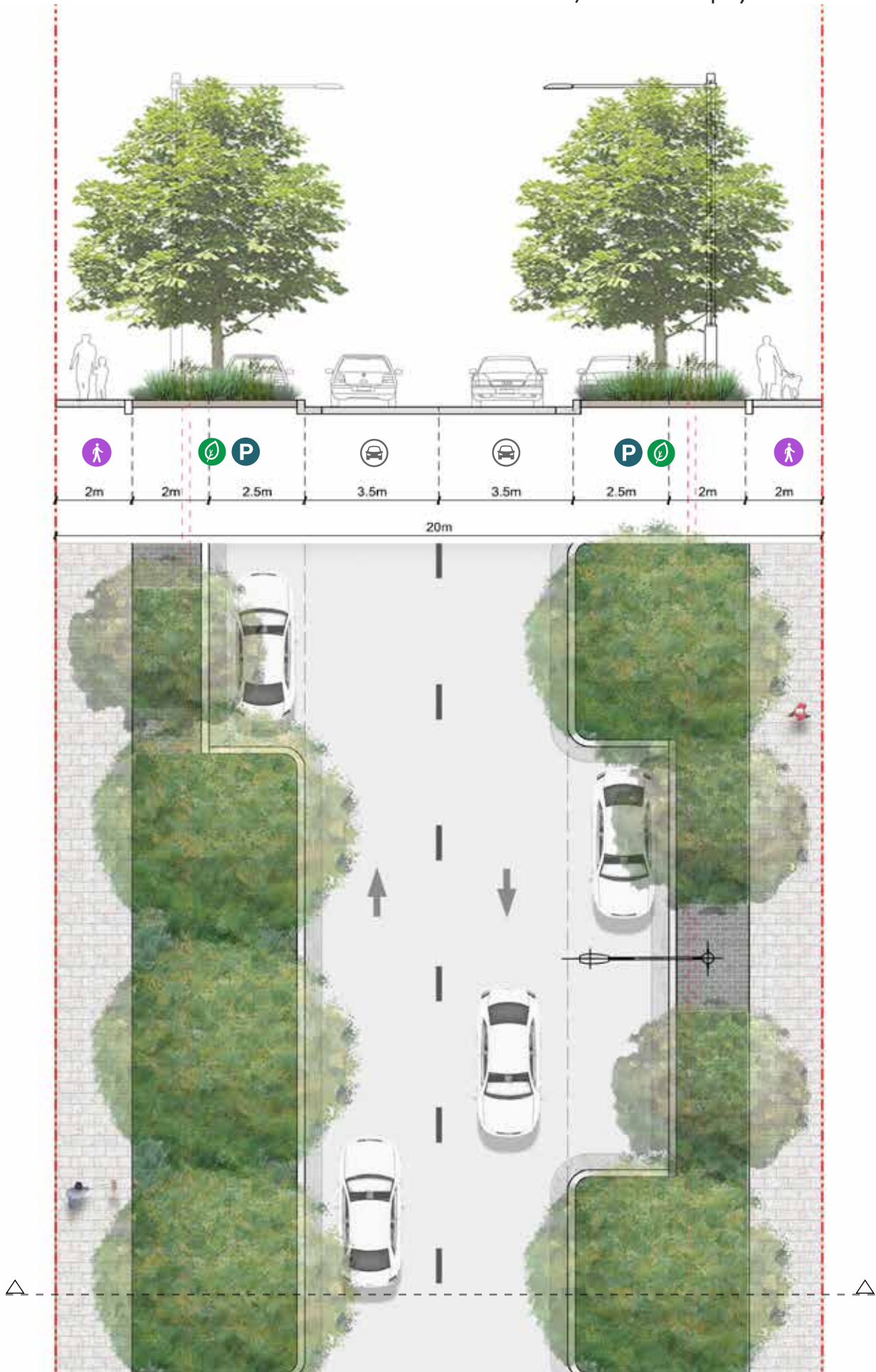


#### Legend

-  Planting
-  Footpath
-  Carriageway
-  Parking
-  Cadastre Boundary
-  Existing kerb line

Key Public Domain Elements	Description
Street Type/ Character	Local Street/ HTC local adaptations
	Medium 30km/h.
	Interface to WSUD plaza to east
Location	Linda Street/ Northern Employment Precinct
Street Geometry	20m road reserve
	Footpath: 2.0m wide in situ brick paving
	Parking: 2.5m wide parallel parking
	Traffic lanes: 3.5m
Streetscape Elements	Footpath precast/ insitu concrete paving as main footpath material with feature detail where appropriate
	Street furniture: bins, and wayfinding signage.
	Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	Street Tree Planting for all new trees in paving. Tree pit to be 4m x 2.5m min with engineered root cells / structural support under hard surfaces.
WSUD features	Not applicable - topography is too steep to support WSUD

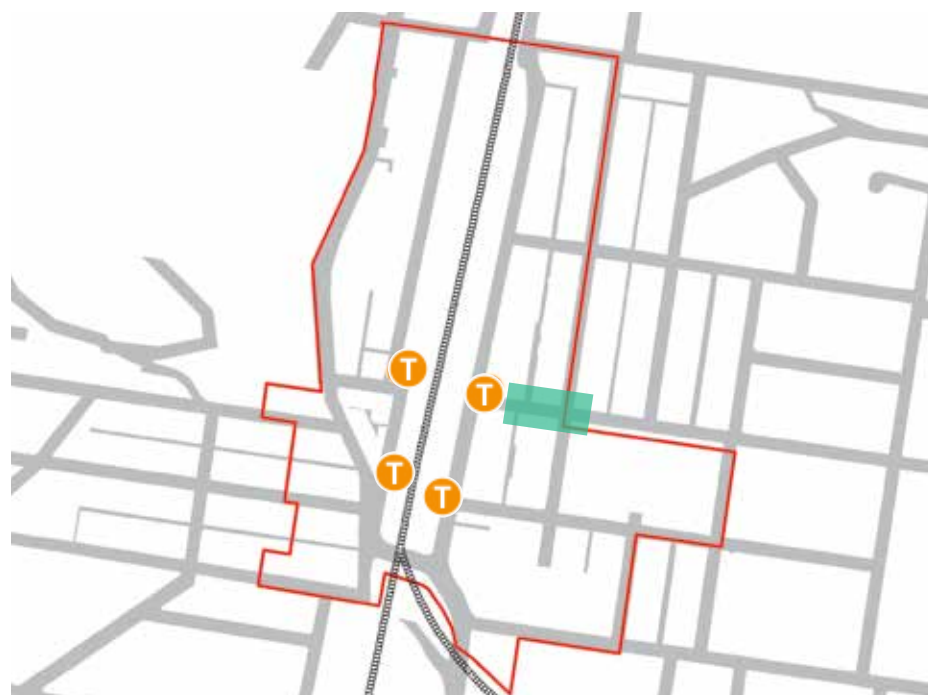










Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
 Concept Only. Final design subject to technical considerations, topography,  
 detailed investigation of services and relevant approvals



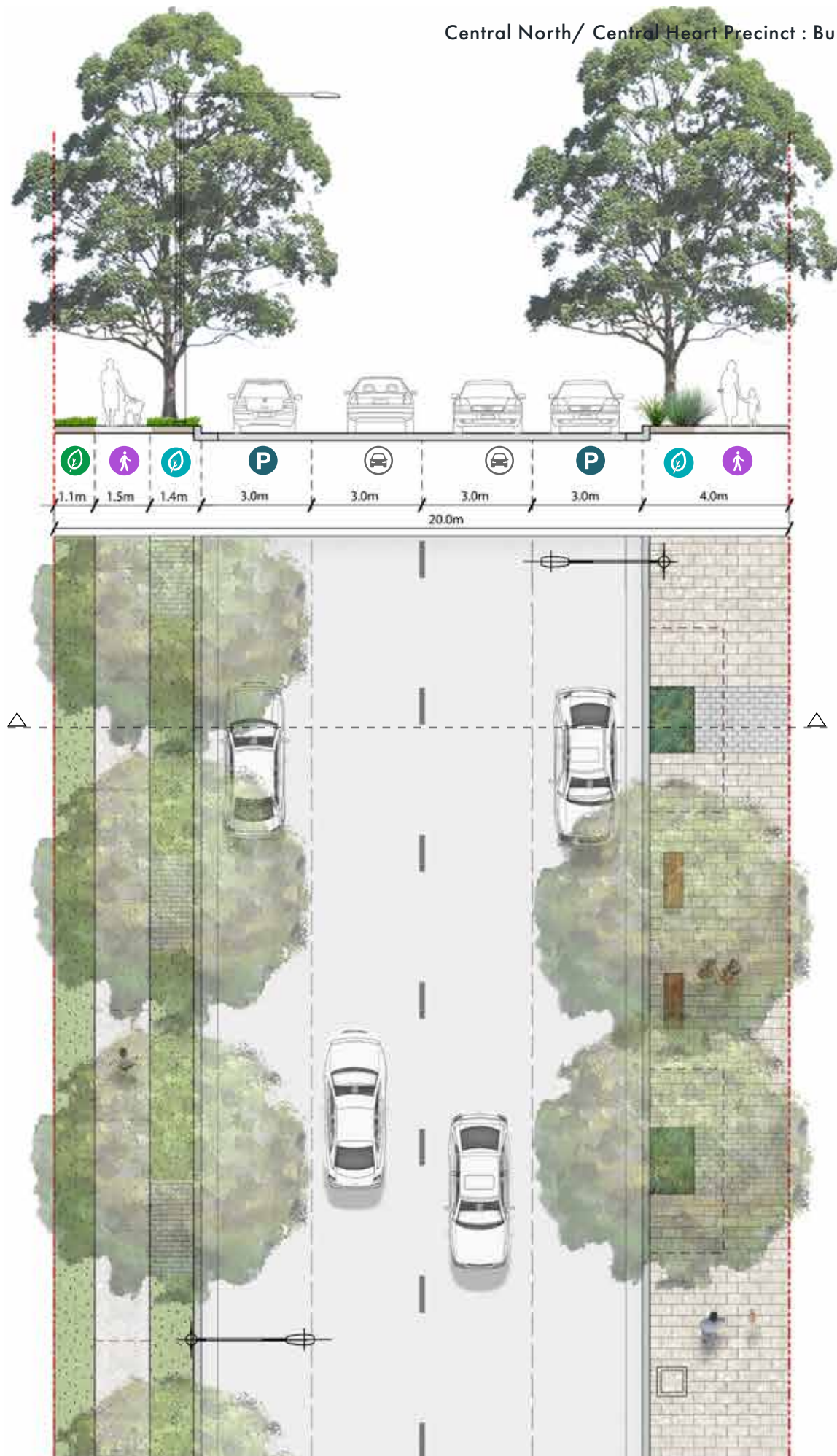
### 5.6.12 Central North/ Central Heart Precinct : Burdett Street



#### Legend

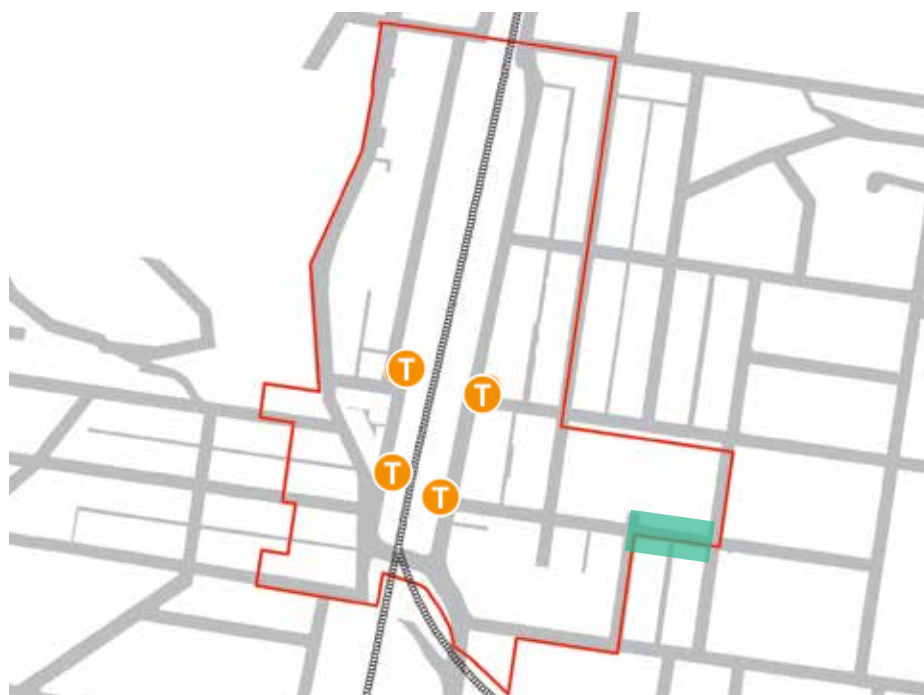
-  Planting
-  WSUD Features
-  Footpath
-  Carriageway
-  Parking
-  Cadastre Boundary

Key Public Domain Elements	Description
Street Type/ Character	Local Street/ Central Heart Precinct
	Medium Zone 30km/h
	Interface between Precincts
Location	Burdett Street
Street Geometry	20m road reserve
	Footpath: 1.5m wide to verge at residential interface
	Footpath: 4.0m to verge at retail/ commercial interface. Porous paving provides pedestrian access.
	Parking: 2.5m wide parallel parking.
Streetscape Elements	Traffic lanes: 3.5m wide
	Footpath: Stone paving to feature areas. High quality precast paving footpath material generally, insitu concrete footpaths to residential interface
	Street furniture: seats, litter bins, bike racks and wayfinding signage. Amenities located along street for pedestrian comfort
Landscape Treatments	Multi function poles: street lights with banner and pedestrian lights
	Grass verge with street tree planting along residential side.
	Street trees in paving to have trench root zone extend under paving with structural support detail to ensure effective root zones available.
WSUD features	Street Tree Planting- for all new trees in paving. Tree pit to be 4m x 2.5m min with engineered root cells/ structural support under hard surfaces.
	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off towards existing trees.








Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
 Concept Only. Final design subject to technical design considerations, topography,  
 detailed investigation of services and relevant approvals

### 5.6.13 Retail Core Precinct : Florence Street

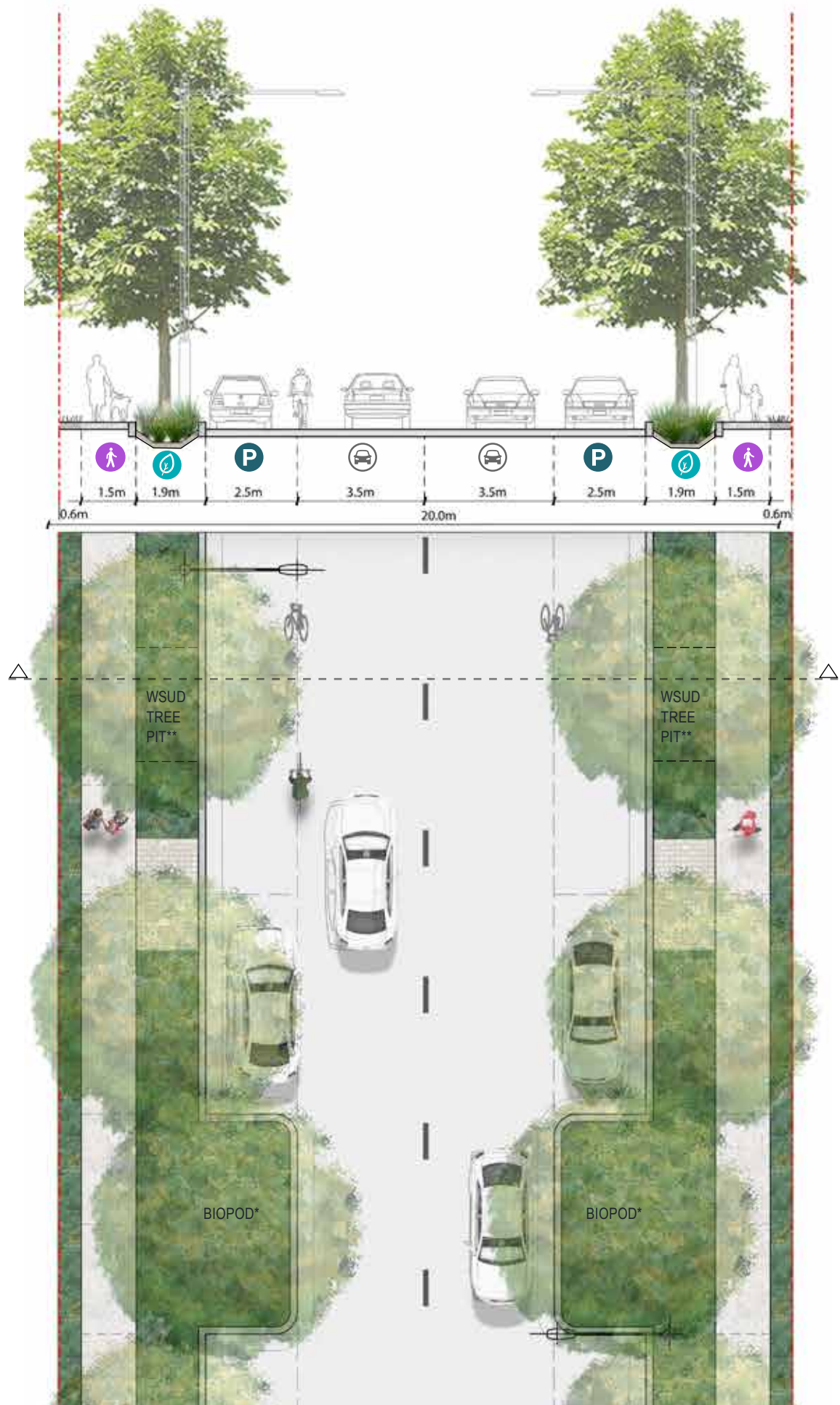


#### Legend

-  Footpath
-  Carriageway
-  Parking
-  WSUD features
-  Cadastre Boundary

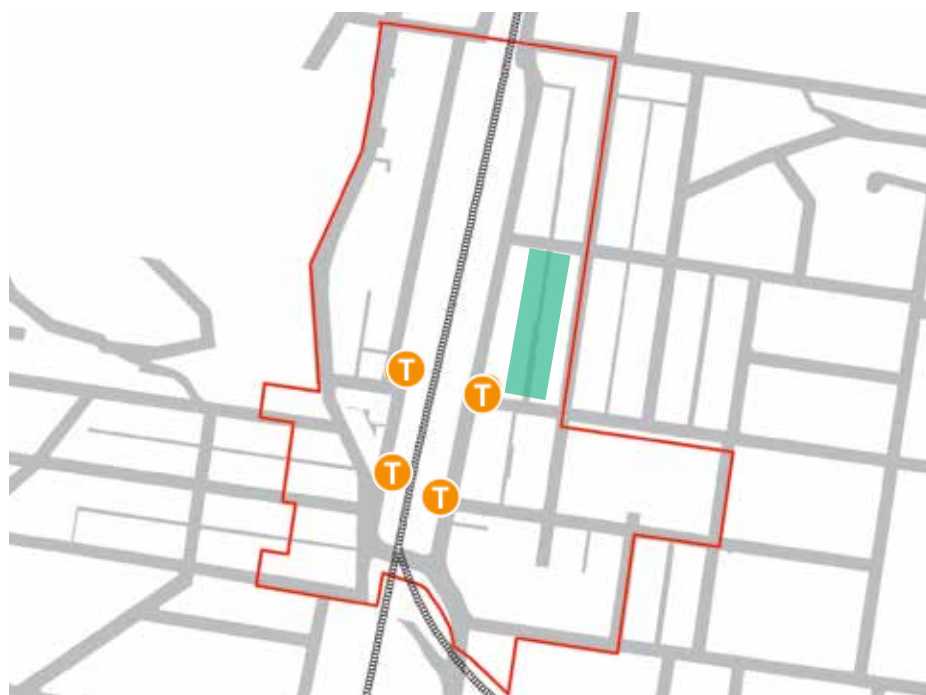
Key Public Domain Elements	Description
Street Type/ Character	Local Street/ Retail Core Precinct
	Medium 30km/h.
	Interface between Precincts
Location	Florence Street
Street Geometry	20m road reserve
	Footpath: 1.5m wide in situ concrete paving. Porous paving provides pedestrian access.
	Parking: 2.5m wide parallel parking
	Traffic lanes: 3.5m
Streetscape Elements	Footpath precast/ insitu concrete paving as main footpath material with feature detail where appropriate
	Street furniture: bins, and wayfinding signage.
	Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	1.9m WSUD verge treatment, with traditional 600mm wide grass clearance strip along boundary.
	Street Tree Planting for all new trees in paving. Tree pit to be 4m x 2.5m min with engineered root cells / structural support under hard surfaces.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off towards existing trees.
	* Biopods 2.5 x 4m in parking lane at 80m spacings. Location to align with street drainage. ** Or tree pits 2m <sup>2</sup> (minimum) at 15cm centres in verge zone as an alternative to Biopods at Council discretion.









Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
 Concept Only. Final design subject to technical design considerations, topography,  
 detailed investigation of services and relevant approvals

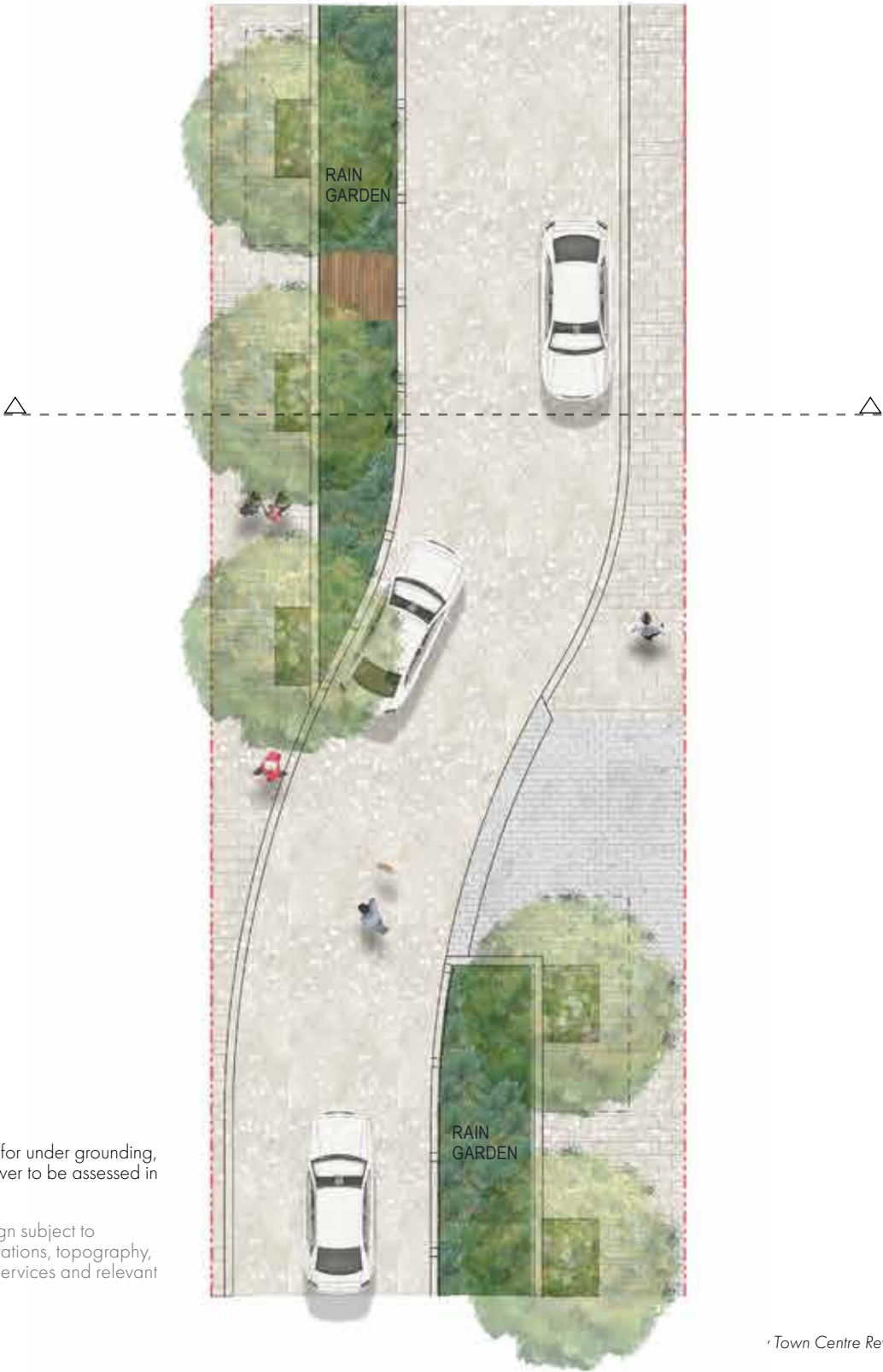
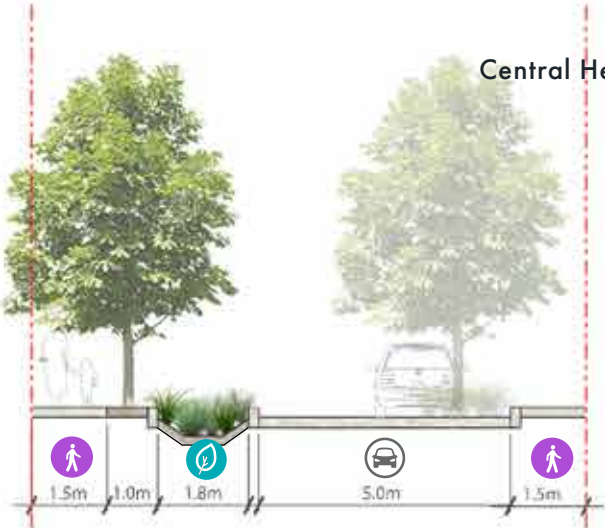
### 5.6.14 Central Heart Precinct : Hunter Lane



#### Legend

-  Footpath
-  Carriageway
-  WSUD Features
-  Cadastre Boundary

Key Public Domain Elements	Description
Street Type/ Character	Laneway
	Slow 20km/h.
Location	Hunter Lane
Street Geometry	11.5m road reserve
	Footpath: 1.5m wide staggered to suit adjoining access requirements
	No Parking
	Traffic lanes: 5.0m two way traffic
Streetscape Elements	Footpath precast/ insitu concrete paving as main footpath material with feature detail where appropriate. Decking feature for pedestrian access path.
	Street furniture: benches, bins, and wayfinding signage.
	Street lights: Laneway scale in staggered arrangement.
Landscape Treatments	WSUD verge treatment,
	Street Tree Planting for all new trees in paving. Tree pit to be 4m x 2.5m min with engineered root cells / structural support under hard surfaces.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	WSUD features have opportunities for: Slotted kerbs to maximise collection of surface water run-off towards existing trees.
	Rain garden in verge.

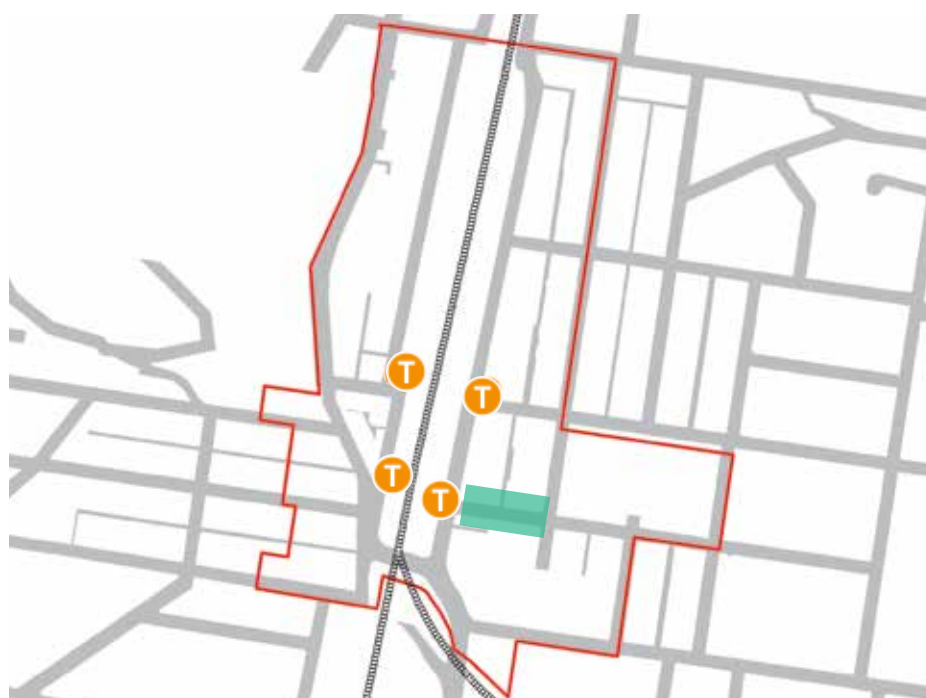


Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street




Concept Only. Final design subject to technical design considerations, topography, detailed investigation of services and relevant approvals



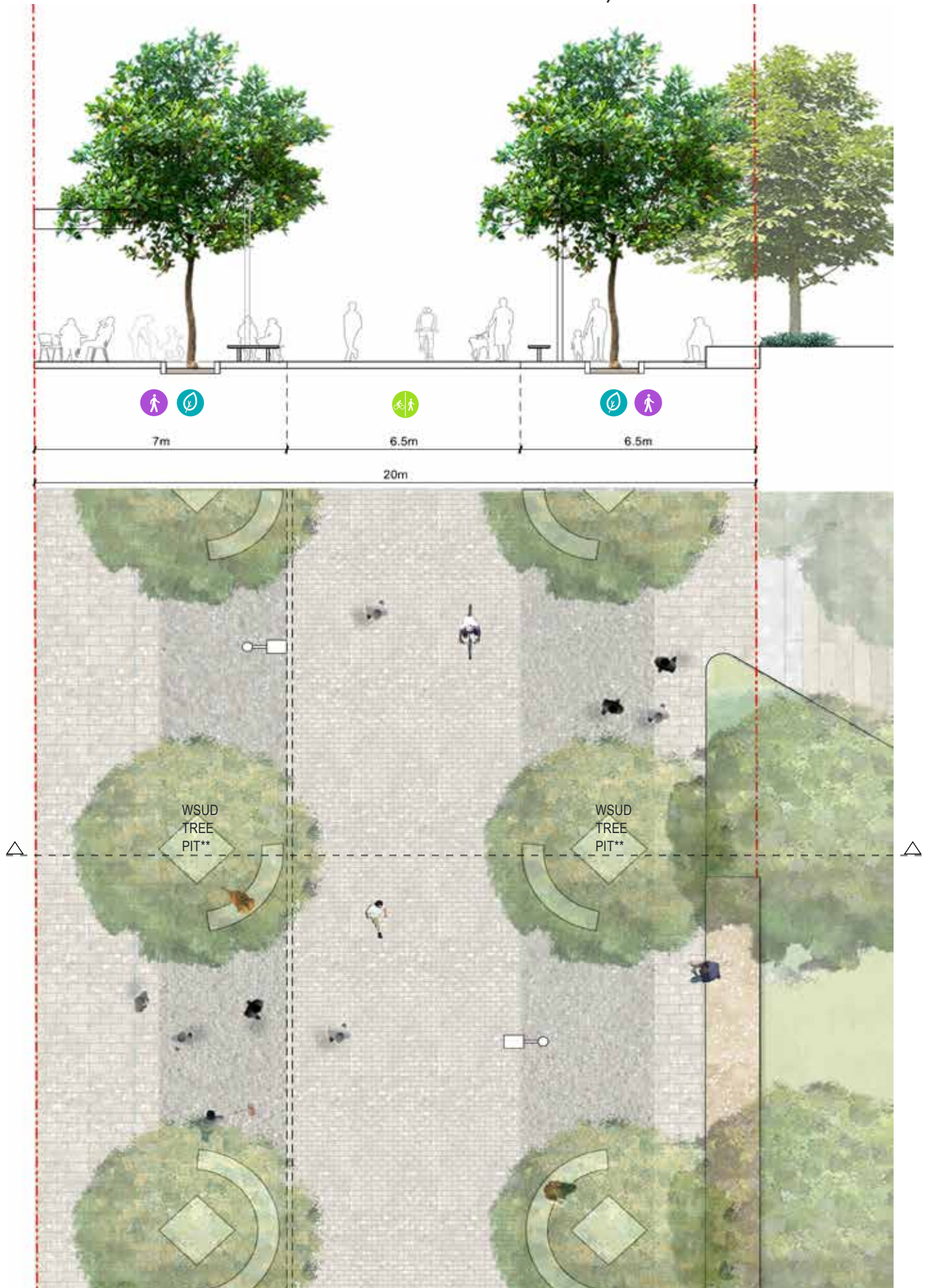
### 5.6.15 Central Heart/ Retail Core Precinct : Florence St - Mall



#### Legend

-  WSUD Features
-  Footpath
-  Shared Path
-  Cadastre Boundary

Key Public Domain Elements	Description
Street Type/ Character	Shared zone - vehicles access only
	Slow 10km/h
Location	Florence St
Street Geometry	20m road reserve
	Footpath: 7m wide to south, 6.5m to north, integrated with street furniture and street trees
	No Parking
	6.5m wide central shared environment
Streetscape Elements	Footpath precast/ insitu concrete paving as main footpath material with feature detail where appropriate, scale differentia for through lane in the middle. Porous paving over tree trenches.
	Street furniture: benches, bins, and wayfinding signage.
	Street lights: Laneway scale in staggered arrangement.
Landscape Treatments	Existing trees
	Street Tree Planting for all new trees in paving. Tree pit to be 4m x 2.5m min with engineered root cells / structural support under hard surfaces.
WSUD features	Final WSUD proposals subject to detail design and as coordinated with street stormwater drainage in each location.
	WSUD detail to be coordinated with street trees in paving to include engineered root cells/ structural soil to ensure effective root zones.
	WSUD features have opportunities for: Flush edges to vegetation to maximise collection of surface water run-off towards existing and proposed trees. Coordinate streetscape stormwater run-off with stormwater harvesting as part of plaza designs **Tree pits 2m <sup>2</sup> (minimum) at 15m centres








Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
 Concept Only. Final design subject to technical design considerations, topography,  
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5.6.16 Retail Core Precinct: Pacific Hwy



Legend

-  Shared Path
-  Footpath
-  Planting
-  Carriageway
-  Cadastre Boundary
-  Existing kerb line

Key Public Domain Elements	Description
Street Type/ Character	Arterial Road - HTC local adaptation
	Medium Zone 50km/h.
Location	Pacific Highway- Retail Core Precinct
Street Geometry	Existing road reserve
	Share Path: 2.5 within land acquisition zone at westside
	No parking
Streetscape Elements	Share path: insitu concrete
	Pedestrian barrier at kerb edge
	Multi function poles: street lights with banner and pedestrian lights
Landscape Treatments	Existing trees
	Street Tree Planting for all new trees in planting bed.
WSUD features	Passive irrigation - surface water run-off from shared path



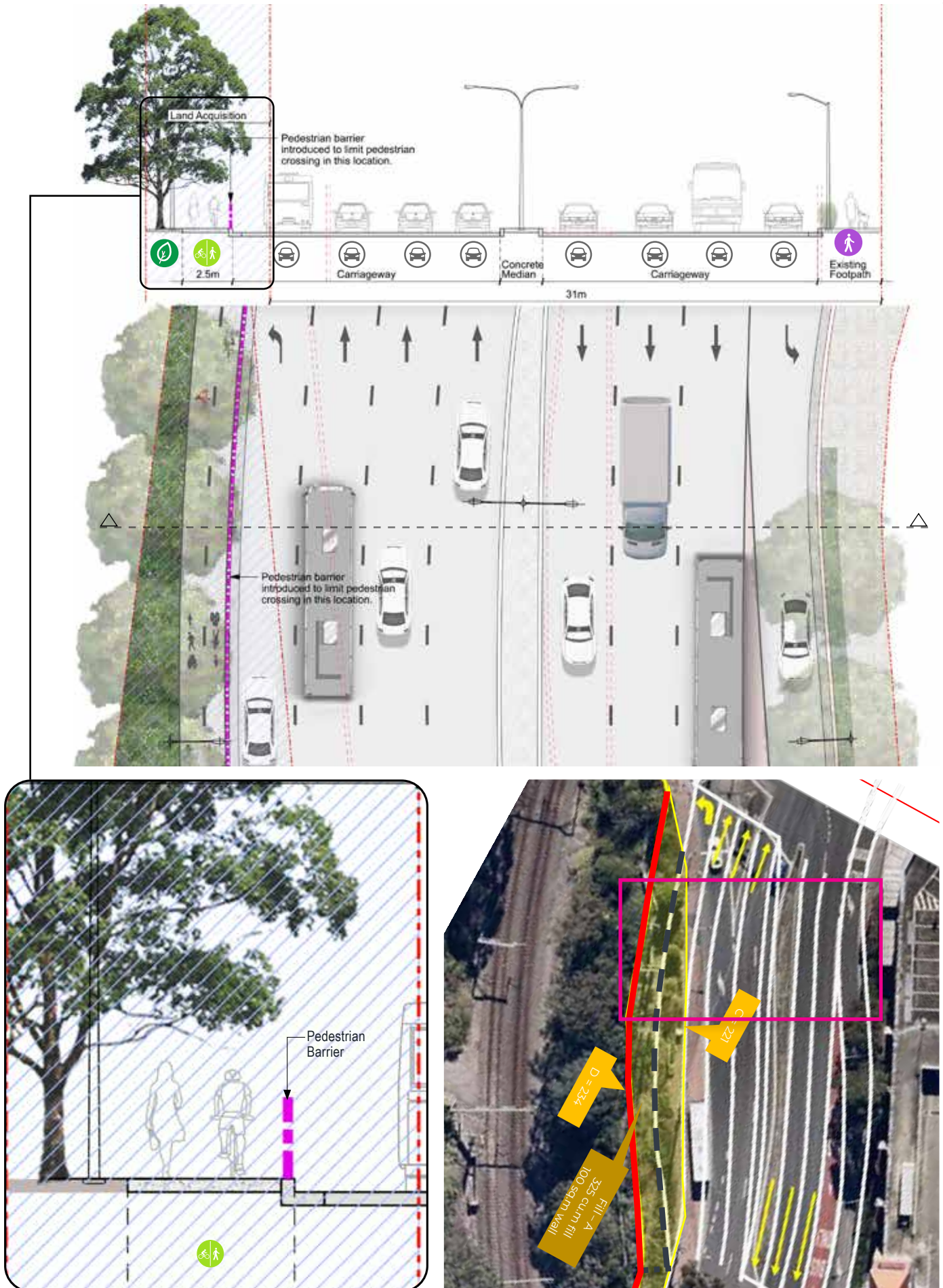


Image extracted from HTRC Transport Plan

Note : Overhead options for under grounding, or bundling overhead power to be assessed in each street  
 Concept Only. Final design subject to technical design considerations, topography,  
 detailed investigation of services and relevant approvals





5.7

PUBLIC DOMAIN KEY  
PLACES & ELEMENTS

MATERIAL  
PALETTES

## 5.7 Material Palettes

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### 5.7.1 Vegetation and Street Tree Selections

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Street trees are a significant component of the urban fabric.

Street trees have the ability to transform the physical appearance of the street, provide environmental, aesthetic and economical benefits.

Vegetation are selected to represent existing local species in locations that symbolically represent their role in a natural landscape - Ridge, gully and transitional areas.

Street trees and vegetation are to be included as a priority in any new streetscape design. This is important for amenity, microclimate and the support of ecological communities, as well as for visual connections and to enhance and promote the Hornsby Town Centre "city in the bush" character.

### 5.7.2 Pavement Design Considerations

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Street pavements are a significant part of the public realm and their quality has a direct effect on the pedestrian experience of a place.

Pavements should reinforce streetscape hierarchy, while being a unifying element in the streetscape.

Pavements should provide clear distinction between pedestrian priority footpaths and vehicle use areas.

Pavements should be comfortable and allow ease of movement for all users including people with different degrees of abilities.

Pavements should be a consistent pattern with occasional textural, size and colour variations to provide relationship to built form and space function, or to alert users of change of conditions or hazards.

Pavement material should be high quality, durable, robust, easy to maintain and are easy to install, remove and relay.

### 5.7.3 Furniture Selection and Other Elements

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Street furniture creates settings for resting, sitting, dining and social gatherings with friends and family. These settings are important for the elderly, less mobile and young families as they provide relief and comfort. Properly selected and placed furniture can encourage people to venture outside and enjoy/activate the public domain.

The furniture palette should be consistent across the Town Centre, and feature bespoke items dedicated to special streets and special places. The main objective is to create easily maintained, convenient and publicly accessible amenities that do not interrupt the pedestrian or traffic flow.

The placement of street furniture should be based on the street function and relate to the patterns and design of the hard landscape elements on site. Street furniture should not give an appearance of being cluttered. Where possible amenities should be grouped and arranged in a linear sequence along a street.

Street furniture should be selected to meet the different needs of users and be constructed from safe materials to prevent injury, without sharp edges or entrapment gaps. Furniture should be securely mounted onto the sub-surface blinding slab to conceal fasteners.

The choice of material should depend on the context and be suitable for the local character. The designs should be simple in form.



#### 5.7.4 Vegetation: Street Tree Masterplan

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The street tree network is designed to create a series of well connected landscape thoroughfares.

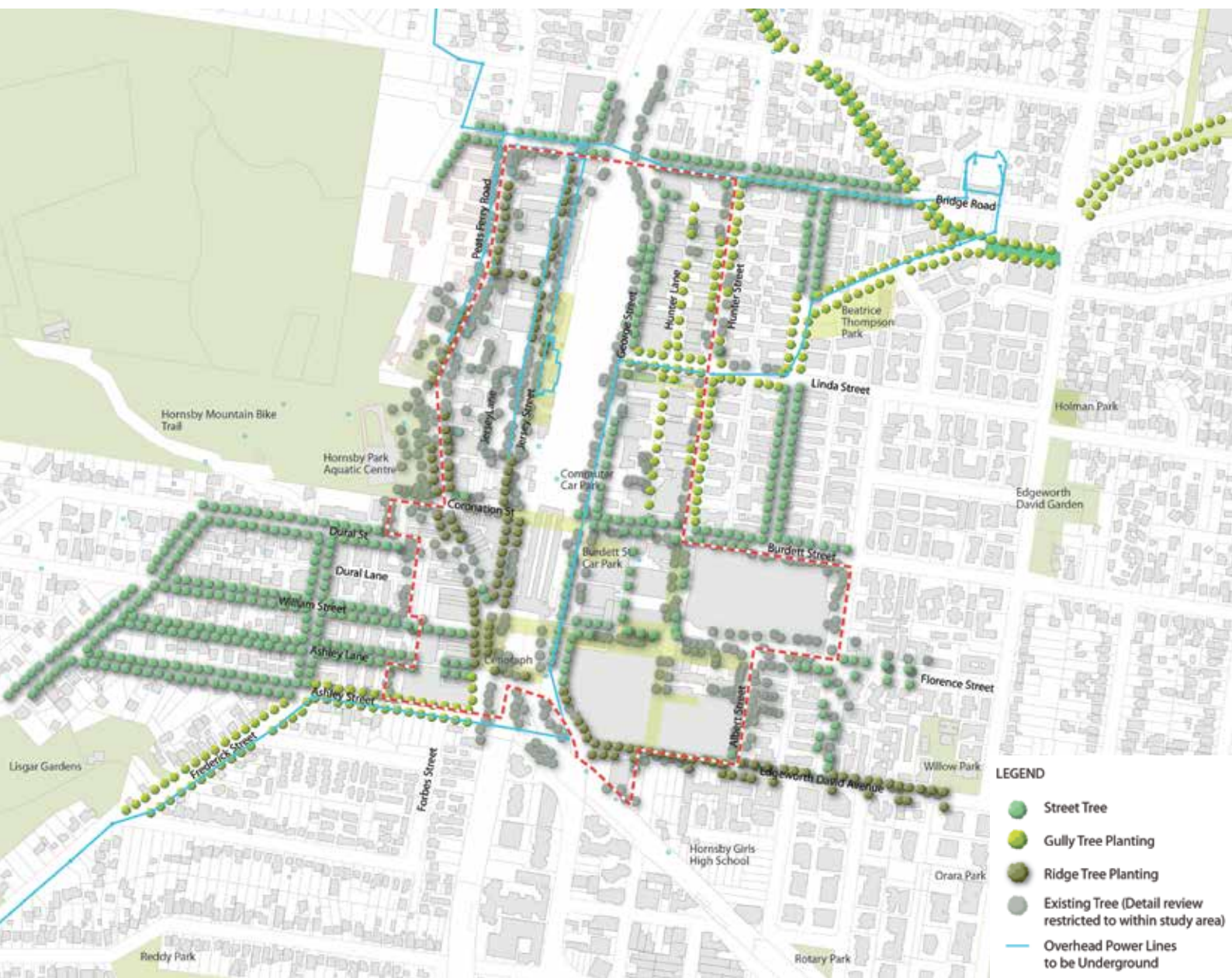
Green corridors will be distinctive, functional and appealing, with a focus on enhancement and showcasing of the local natural biodiversity, particularly the opportunity to bring physical and visual representation of the bushland setting.

Overhead powerlines are accepted as a constraint. Powerlines are mapped to inform and identify benefit to the street tree network should future development take the opportunity to underground them.

Supplementing the existing street tree network with infill tree planting will assist with physical and visual connections to the town centre surrounds, as well as enhancing the immediate street experience and amenity.

Selection of street trees species in each locations relates to:

- Environmental and Ecology overlay
- Immediate context and functionality
- Opportunities to enhance vegetation links



#### LEGEND

- Street Tree
- Gully Tree Planting
- Ridge Tree Planting
- Existing Tree (Detail review restricted to within study area)
- Overhead Power Lines to be Underground

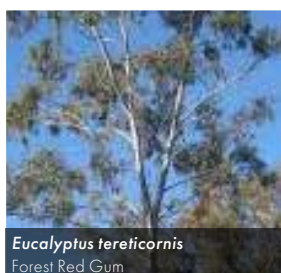
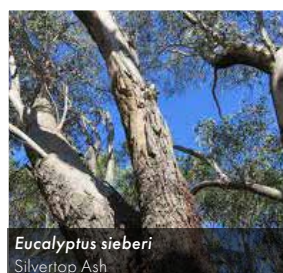
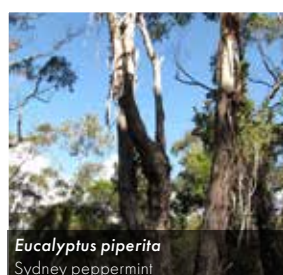
0 50 100 200m



### 5.7.5 Vegetation: Ridgeline



#### Indicative Ridgeline Species List





## 5.7.6 Vegetation: Gully

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### Indicative Gully Species List

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*Acmena smithii*  
Common Lilly Pilly



*Alphitonia excelsa*  
Red Ash



*Backhousia myrtifolia*  
Grey Myrtle



*Callicoma serratifolia*  
Black Wattle



*Ceratopetalum apetalum*  
Coachwood



*Ceratopetalum gummiiferum*  
NSW Christmas Bush



*Cryptocarya glaucescens*  
Jackwood



*Doryphora sassafras*  
Sassafras



*Elaeocarpus reticulatus*  
Blueberry Ash



*Eucalyptus saligna*  
Sydney Blue Gum



*Syzygium australe*  
Brush Cherry



*Syzygium oleosum*  
Blue Lillypilly



*Syzygium paniculatum*  
Magenta Lillypilly



*Toona australis*  
Australian Red Cedar



*Tristania laurina*  
Water Gum

### 5.7.7 Vegetation: WSUD Streets / Plazas

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### Indicative Swampland / WSUD Species List

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*Carex appressa*  
Tall Sedge



*Ficinia nodosa*  
Knobby Club Rush



*Ghania clarkei*  
Tall Saw Sedge



*Juncus usitatus*  
Common Rush



*Lomandra longifolia*  
Spiny-headed Mat-rush

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### 5.7.8 Streetscape Pavement Materials Masterplan

- Material selections across the Town Centre respond to the nature and focus of each precinct, being heritage, retail, civic, commercial and residential.
- Materials will be easy to repair, repaint and replace over time, minimising maintenance related hazards.
- Pavement and hard surface materials support the:
  - useability and legibility of spaces that can be interpreted by all users;
- streetscape and open space structure plan hierarchy;
- functional requirements for each street and open space area
- influence the character and experience of public domain areas; and
- orientation and wayfinding throughout the town centre.



### 5.7.9 Wayfinding Strategy Plan

Town centre wayfinding elements will be multilayered, supporting identity, orientation, journey and behaviour throughout the town centre public domain.

Distinctive architecture and public places create memorable landmarks which support orientation within the town centre. In addition streetscape design supports intuitive wayfinding in a number of ways:

- Ensure wayfinding is accessible for all and dementia friendly, with large signage and clear colour contrast, and utilising visual landmarks.

- guided movement through the town centre towards destinations, strengthened using vistas and views;
- establish character and identity for neighbourhoods through vegetation and materials;
- reinforce the town centre structural layout; and
- support appropriate behaviour through clarity of use of materials.

A suite of appropriate wayfinding elements will guide journey, behaviour, offer interpretation and share knowledge/ education.

Wayfinding strategies can also enhance the public domain experience where includes cultural orientation points, celebration of knowledge, language, and story. This can be further supported by lighting strategies which can emphasise nighttime identity, direction, journey and gathering area.

Detailed design of town centre wayfinding elements will be in accordance with HSC Signage and Wayfinding Strategy.



### 5.7.10 Pavements and Pedestrian Hard Surfaces\*

Large format surfacing, Community plazas and gathering spaces

- Granite pavers, textured finishes
- Be-spoke and complimentary pre-cast concrete pavers



Left: Parramatta Square - Tract  
Right: Granite Paver - Tract

Medium format surfacing, streets and pedestrian connections

- Pre-cast concrete pavers



Left: Wynyard Walk - Tract  
Middle: Darling Harbour Park - Tract  
Right: Darling Harbour - Tract

Small format surfacing, laneways, contrasting detail and spaces with heritage/ central precinct focus

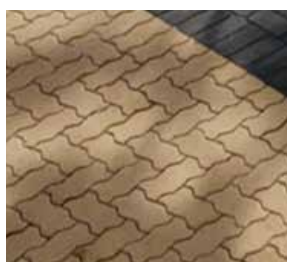
- Brick pavers
- Small unit pre-cast concrete pavers



Left: Reclaimed Antique Paver - Historicalbricks  
Middle: Permeable Pavers - Pinterest  
Right: Parramatta Park - Pinterest

Alternative vehicle surface material - small scale and contrasting colours/ textures used in areas where pedestrian environment prioritised over vehicle. Station Street / Precinct, raised vehicle thresholds, and gateway

- Small unit pre-cast concrete pavers



Left / Middle: Concrete interlocking paver - AdbriMasonry

\* Indicative design only.  
Delivery to be consistent with Council specifications.



### 5.7.11 Street Furniture\*

#### Bench Seats

Streets and pedestrian connection,  
community plaza and gathering  
spaces

Simple and elegant bench seating  
which meet equitable access  
requirements.



Top Left: Hub Street Furniture  
Middle: Mos Urban  
Right: Parramatta Park - Pinterest

#### Bike Racks

#### Bollards

#### Drinking Fountain and Re-fill stations

#### Multi Function Poles

Streets and pedestrian connection,  
community plaza and gathering  
spaces



Top Left: Bikerack - Tract  
Middle: Bollard - Street Furniture Australia  
Right: Drinking Fountain - Street Furniture Australia  
Bottom Left: Multi Function Pole - Multipole

#### Litter Bins and Recycling Centres

Streets and pedestrian connection,  
community plaza and gathering  
spaces



Left: Mos Urban  
Right: CSA

\* Indicative design only.  
Delivery to be consistent with  
Council specifications.

## 5.7.12 Place Based Elements\*

Cycle share paths though high pedestrian areas  
Streets and high pedestrian traffic areas.

- Line-marking
- Colour contrast materials



Interpretive Paving  
Streets, laneways, community plazas and gathering spaces

- Story telling and knowledge sharing
- Opportunity for art integration



Top Left, Top Middle, Top Right: Tract  
Bottom Left: Tract  
Bottom Right: Pinterest

Shelters and bespoke furniture  
Community plazas and gathering spaces

- Provide orientation and town centre signature
- Opportunity for art integration



Bespoke seating and level change elements  
Signature elements, with a suite of related walls, secondary seating and stand alone seating elements



Left: Darling Quarter Sun Shade - Flickr  
Middle: Arch Daily  
Right: Landezine

Left: City Walk: Tract  
Right: Landezine

\* Indicative design only.  
Delivery to be consistent with Council specifications.

### 5.7.13 Play Elements\*

Formal play equipment  
Urban style play elements, selections to consider requirement for all abilities play, pre-school and primary school age groups.



Multi use sports play areas  
Multi-use courts to be adaptable and flexible, with additional markings to allow for solo practice. Includes table tennis tables and kick-a-bout space.



Heritage referenced play spaces

Play spaces which incorporate art elements or re-use of heritage artefacts to share stories about Hornsby's rich history



Water play  
Inclusion of water jets and adapted equipment are a small inclusion with a big impact  
Programmable to allow for controlled use at appropriate times.  
Creates memorable destinations in town centre.



Top Left: Play Concept - Pinterest  
Top Middle: Urban Play - Pinterest  
Top Left: Park in Badalona - Pinterest  
2nd Row Left: Box Hill - Pinterest  
2nd Row Middle: University Campus - Pinterest  
2nd Row Right: University Campus - Pinterest  
3rd Row Left: Pavilion Architecture - ArchDaily  
3rd Row Middle: Pak Tsz Lane Park - Archinect  
3rd Row Right: Sub Base Platypus - Bumpintomums  
Bottom left: Water Play - Pinterest  
Bottom Right: Pirrama Park - MidCoast

\* Indicative design only.  
Delivery to be consistent with Council specifications.



### 5.7.14 Green Walls and Living Elements\*

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#### Green Walls

Community plazas, screening elements, barriers and fence element.

- Barrier or screening edge detail
- Orientation and view screening
- Story telling and knowledge sharing



#### Living sculpture

Community plazas and gathering spaces

- Interpretive play
- Orientation and way-finding
- Story telling and knowledge sharing
- Opportunity for art integration



Top Left: Green Wall - Pinterest

Top Middle: Green Facade - Pinterest

Top Left: Green Wall - Pinterest

Left: Arbor- Greenscreen

Middle: Pinterest

Right: Parklet - Pinterest

## 5.7.15 Wayfinding Elements\*

### Gateway Threshold Markers

- Distinctive facades/ architecture
- Landmark public art
- Iconic signage



### Destination & Interpretation Markers:

- Clearly identifiable marker points.
- "You are here" mapping and physical precinct guides.



### Directional Signs

- Pedestrian scaled journey markers
- Build on established wall art themes.
- Opportunity to integrate interpretation.



### Plaza and gathering spaces -

- Interpretation opportunity
- Info boards
- Art items
- Integration into materials and furniture



### Behaviour management and mandatory signage

- Graphic and text guides to share behaviours and expectations
- Standard regulatory signage



\* Indicative design only.  
Delivery to be consistent with Council specifications.

### 5.7.16 Water Sensitive Urban Design - Street Elements\*

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Tree pits / garden bed with slotted kerb



Sunken garden bed integrated with footpath



Permeable paving



Rain garden and Bioswale



Biopods/ tree pits



\* Indicative design only.  
Delivery to be consistent with  
Council specifications.

Source: DesignFlow



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